



**MANUAL OF
FOUR WHEEL DRIVE
MOTOR SPORT
FOR
FOUR WHEEL DRIVE
COMPETITIVE EVENTS**

Ver 11

(17 July 2021)

Note: Check Revision History for change details

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This Manual of Four Wheel Drive Motor Sport is published by the Cross Country Drivers Association Inc. Enquiries should be directed to the Secretary, PO Box 175, Bulleen, VIC. 3105, Australia.

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Many people have contributed to the development and publication of the National Rules. The contribution of event organisers, competition officials and marshals, and competition drivers and co-drivers is acknowledged with thanks.

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MANUAL OF 4WD MOTOR SPORT

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NOTE:

Each section above is available as a separate document on the CCDA website:

www.CCDA.com.au

Revision History		
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4.0	19-Jan-2005	Initial version of document for public release.
4.1	11-Mar-2005	Interim changes to reflect comments raised after the Marshal training program.
4.2	03-Apr-2005	Further changes post Marshal practical training.
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9.0	20-Jan-2009	Production Class Specification changes, Exhaust System, Winch Hook

Revision History

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10.0	1-Jan-2012	Licence, Abuse of Alcohol, Vehicles, Technical Inspection, Vehicle Scrutineering, Event Scrutineering, 2.33.5 Safe Recovery Setup, Fire Equipment, Advertising Decals, Occupant Protection, Window Nets, Code of Conduct, Scoring, Bar work, Recovery Points, Cabin, Batteries, Production/Challenge/Open Class, Cable Storage, Fuel Storage, Ready to Start Condition
10.1	22 nd Jan 2014	Non-competitive speed limit, removal of registration requirement (Challenge Class), unloading area, pit area. Non-competitive speeding penalties.
11	26 Jan 2018	Document rehashed and formatting fixed, Alcohol Policy updated, Recovery rules moved to sporting regulations. Terms standardised. Window net penalty specified. Parc ferme definition added. Fuel Containers. 2016 rule changes added/revised/deleted to: All classes – Tyres, General Comments-Purpose-Scrutineering, Batteries, Fuel (Challenge Class), Group D scoring, Presentation of Awards, Lights, Winches, Removed references and penalties related to wire rope, Supplementary rules and regulations. Fire extinguisher requirements Marshal Roles added
11.1	24 August 2020	<i>Document broken into separate doc files.</i> ROPS (Roll Cage) rules revised, Protest point deposit defined, Chassis requirements defined for Challenge and Open Classes, Recovery point yellow colour added, Broken windscreen requirements added, Caution warning sign requirements added, Emergency/Hand brake also defined as Park brake. Crew substitution penalty standardised. Chassis definition clarified. CCDA ROPS label location specified
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11.3	25 April 2023	Weekend Warrior Class added, Wheel spacers allowed for Challenge class vehicles

Section update schedule

The following table details the current manual documents

Section	Revision no	Date
1	11.3	25 April 2023
2	11.2	17 July 2021
3	11.1	24 August 2020
4	11.3	25 April 2023
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SECTION 1 – JURISDICTION AND ADMINISTRATION



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1 JURISDICTION AND ADMINISTRATION

1.1 NATIONAL RULES

This Manual sets out the National Rules for four wheel drive competitive events conducted under the auspices of the Cross Country Driver's Association.

The National Rules set out the conditions and regulations under which CCDA sanctioned events are to be conducted. The National Rules were established by a representative group of Event Organisers, Officials and Competitors with the aim of providing a safe and regulated environment under which to conduct four wheel drive competitive events.

1.2 PHILOSOPHY OF NATIONAL RULES

These rules do not purport to be directly applicable to every situation that may be encountered in any form of four wheel drive competitive event.

Practical and sensible application of the spirit of these rules requires all participants to work together to ensure, as far as possible, that competition is fair and to use discretion and good sense in the application and interpretation of these rules.

Officials and marshals should exercise judgement wisely and fairly in a way that supports the philosophy of the rules and understand that their decisions are subject to protest and appeal.

1.3 PROCESS FOR UPDATING THESE RULES

CCDA members may propose amendments or additions to these National Rules in a submission to their State or Territory Committee.

The submission should clearly state the proposed amendment or addition and briefly explain the reasons for the proposal. An example of a circumstance under which the proposed amendment or addition may be applied may also be useful in supporting the proposal.

The State or Territory Committee shall receive any proposed amendment or addition and review the proposal, meeting with the proponent as necessary to ensure the proposal is clearly set out, and to assess whether the situation under which the proposed amendment or addition may be applied may already be subject to an existing rule.

The State or Territory Committee shall then forward the original proposal with a report of their review and assessment to the CCDA National Executive, which shall examine the proposed amendment or addition, and the report of the State or Territory Committee and shall take expert advice where appropriate.

The National Executive shall circulate their recommendation for either adoption or rejection of the proposed amendment or addition to the members of the CCDA, via the web site, for comment with a 21 day limit and on receipt of comments review their recommendation for submission to the Executive Committee and, if the recommendation is for adoption, the date recommended for implementation.

The National Executive shall ensure that amendments or additions to these National Rules are formally incorporated into these National Rules.

The outcome of the National Executive deliberations shall be advised to the State and Territory Committees and the proponent member.

MANUAL OF 4WD MOTOR SPORT
SECTION 2 – STANDING REGULATIONS



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2 STANDING REGULATIONS

2.1 INTRODUCTION

Cross Country Drivers Association (CCDA) events are a test of driving skill, vehicle reliability and capability, vehicle recovery, navigation and individual skills, taking place on public and/or private land.

Cross Country Four Wheel Drive events involve negotiation of difficult terrain such as contrived and natural obstacles and difficulties including vehicle recovery techniques. Emphasis is on vehicle capability and driver ability rather than speed.

The Cross Country Drivers Association Standing Regulations, Sporting Regulations and Vehicle Specifications shall be augmented by Supplementary Regulations for an Event and all documents shall be read together.

2.2 THE SPIRIT OF THE REGULATIONS

The regulations shall apply to four-wheel drive vehicles in standard or production configuration, or modified as provided in the Vehicle Specifications, successfully negotiating various terrains under competitive conditions. The use of additional track building aids such as sand ladders and synthetic matting shall not be permitted in CCDA sanctioned events.

Any eligibility matter relating thereto shall be interpreted and applied in accordance with this spirit.

2.3 CCDA MEMBERSHIP

Competitors, officials, marshals and media personnel participating in CCDA sanctioned Events shall be current members of the CCDA.

2.4 LICENSES

Each driver shall hold a current, valid (not expired, disqualified, cancelled or under suspension) State, Territory or International driver's licence (including probationary licence). Navigator must hold a minimum of a current Learner's Permit or Probationary Licence.

2.5 CREWS

Crews shall be a minimum of two people in all Events and Stages and be designated as:

- Driver - who may drive and/or navigate.
- Co-Driver – who may drive and/or navigate but is commonly the navigator at the start of the Event.
- Navigator – who may only navigate.
- Support Crew or Pit Crew – who may only help with any tasks other than driving or navigating.

The Supplementary Regulations for each Event shall specify the maximum number of Crew to compete in each vehicle.

2.6 CODE OF CONDUCT

At all times Competitors and Event Officials shall treat all participants in a courteous and respectful manner.

Competitors shall avoid public argument with each other and officials and all disputes shall be dealt with under the provision for Protests in the Sporting Regulations.

Competitors shall abide by an official's instructions.

Verbal or physical abuse by any participant shall attract a penalty.

Any competitor or participant who is found guilty of a serious breach of the code of conduct may be disqualified from further participation in the Event.

2.7 ABUSE OF ALCOHOL OR DRUGS

The abuse of alcohol or drugs by Competitors or Event Officials at any time during an Event shall not be permitted. Medication prescribed for a Competitor by a doctor shall be accepted, provided such medication does not affect that Competitor's safe performance during the Event. Prescribed medication shall only be taken by the Competitor for whom it is prescribed.

Alcohol shall not be consumed prior to, or during, any competition stages, or before departing for, or during, any transport stages.

2.7.1 MANDATORY TESTING:

Mandatory blood alcohol content (BAC) testing will be carried out at all CCDA events on all days of competition utilising an Australian standards approved BAC testing device, in accordance to the devices operating instructions and hygiene guidelines.

A BAC testing station will be setup at an advertised location at the event (generally near event control) and manned by CCDA or event approved personnel. The station will be opened 45 minutes prior to driver's briefing each day. Long course events may modify the location and time of testing to suit logistical limitations present in those type of events. The time & place of BAC testing will be advised in supplementary regulations and/or drivers briefing.

Drivers and co-drivers (navigators) must present at the BAC testing station to undertake a primary (vapor) test. this is a non-contact test requiring the subject to count or speak into the machine to detect any alcohol present on the breath.

Both driver and Co-Driver (navigator) must maintain a 0.00 reading at all times that they are still competing in open stages.

Upon a negative result:

The individual will be given a wristband and/or stamp to identify a 0.00 reading for that day. This wristband is to remain on whilst racing and only to be removed at the completion of the day's event or upon returning a positive result at a random breath test throughout the competition.

Marshalls and officials will be advised not to let any team complete a stage if both driver and navigator do not have their wristbands intact.

The individual must record their reading and sign the register at the station.

Upon a positive result:

The Individual must complete a secondary breath test (blowing into the machine directly). If at this point a reading greater than 0.00% is recorded this must be entered into the register and then signed. The team will then incur a 100 point penalty.

After a positive secondary test the individual will have a period of 30 minutes to complete a re-test. If at this time a secondary test is greater than 0.00% then they are disqualified from that day's racing.

If the individual does not present within 30 minutes for a re-test, they are disqualified from the day's racing.

2.7.2 RANDOM TESTING:

Drivers/co-drivers:

At any time during competition time, while wearing a wristband/stamp as per above you are requested to complete a breath test by an official you must do so.

A positive result above 0.00% BAC will result in immediate disqualification from the remainder of the day's racing.

Refusal will result in immediate disqualification from the event.

Upon completion of the days competitive events removal of the wristband/stamp indicates you have finished racing for that day.

Pit crew:

At any time you are in control of a vehicle and requested to complete a breath test by an official you must do so. BAC limits are in line with the relevant road safety act. (e.g. 0.05 for fully licensed drivers and 0.00 for restricted drivers) If you return a BAC above these limits you will incur a 200 point penalty for your registered team on the first occasion.

On a second occasion of any pit crew within the same team, the team will incur a second 200 point penalty and be disqualified from the event.

This rule applies to anyone onsite operating a vehicle non competitively.

NB: The CCDA suggests at any time you may be over your relevant limit you simply seek another form of transportation.

Officials, Marshals or anyone operating a vehicle onsite other than a competitor vehicle on a race stage:

At any time you are requested to complete a breath test by an official you must do so. BAC limits are in line with the relevant road safety act. (e.g. 0.05 for fully licensed drivers and 0.00 for restricted drivers) If you return a BAC above these limits you may be removed from the event site when safe to do so.

** Event organisers may apply additional restriction on alcohol consumption above and beyond this policy.

Any Event Official or Competitor who suspects another Event Official or Competitor to be under the influence of drugs or alcohol shall refer the matter immediately to the Event Organiser.

2.8 NON-COMPETITIVE COMPETITION VEHICLE MOVEMENTS

All competing vehicles must refrain from driving on the competition sites when not essential to do so. Transport to and from stages and the pit area, and between stages are acceptable. Using the vehicle as general transport for people or goods around the competition site is prohibited.

2.9 NON-COMPETITIVE SPEED LIMIT

Any competition or recovery vehicle that is underway on the competition site when not competing must do so at a maximum speed of 5 km/h (walking pace). If the vehicle must traverse an area where spectators or public are present, an official or marshal must chaperone the vehicle through this area. The driver must stop in a safe place and radio the Competition Manager or Stage

Manager to arrange for an official or marshal to assist before proceeding. If a competition vehicle is observed to be speeding on the competition site a 20 point penalty will apply on the first offence, a 100 point penalty will apply for any subsequent offences. The Competition Manager reserves the right to disqualify any team who repetitively breaks the speed limit or causes undue risk to any other person.

2.10 VEHICLE UNLOADING AND LOADING

Competition vehicles must be loaded and unloaded only in the area designated to do so. This area shall be fenced or bunted and public, spectators and non-essential persons prohibited entry. Vehicles must then be moved to the pit area via the most direct route while adhering to the non-competitive speed limit.

2.11 CREW SUBSTITUTION

The original Crew as advised on the Event entry form may only be changed with the approval of the Event Organiser. Any Crew which includes a person not identified to and approved by the Event Organiser shall be disqualified. Substitution of a driver or navigator shall incur a penalty of 100 points for each substitution over the course of an event.

2.12 SELECTION OF COMPETITORS

Supplementary Regulations shall specify the means of selection of Competitors. The selection method shall be based on the competency and/or experience of applicants and shall be commensurate with the degree of difficulty anticipated for the Event.

2.13 VEHICLES

A high standard of presentation shall be insisted upon at all times. Any vehicle considered to be of an inappropriate standard of preparation may not be permitted to start, at the discretion of the Event Scrutineer. A vehicle that is accepted by the Event Scrutineer shall not be substituted.

2.14 ELIGIBILITY

The Event Organiser may invite entries for vehicles belonging to any or all of the following classes as defined by the Vehicle Specifications. Vehicles may only compete in the Event Group specified

GROUP A EVENTS:

Standard Class.
Touring Class.
Production Class.
Modified Production Class.
Challenge Class.

GROUP B EVENTS:

Touring Class.
Production Class.
Modified Production Class.

Challenge Class.

GROUP C EVENTS:

Production Class.
Modified Production Class.
Challenge Class. Open Class

GROUP D EVENTS:

Production Class.
Modified Production Class.
Challenge Class.

Event Supplementary Regulations shall specify the vehicle classes permitted for that Event.

2.15 EVENT GROUPING

Events shall be grouped by the nature of the terrain and the degree of difficulty of the exercises to be performed. The CCDA shall approve the grouping for each event.

2.15.1 GROUP A EVENTS

Group A Events shall include traversing open public roads and shall not be timed. Competitors and vehicles travelling on public roads shall comply with all applicable statutory rules and regulations.

2.15.1.1 NAVIGATION TRIAL

Navigation Trials may incorporate nominated destinations, clue solving, observation, and map reading. They shall not be timed and may provide for multiple vehicle teams. Several competing vehicles or teams may be on the course simultaneously and there may be manned and unmanned checkpoints.

2.15.1.2 OBSERVED SECTION

Observed Sections may incorporate muddy, rocky and slippery surfaces over steep and difficult terrain. The task shall be to traverse the whole of the Stage non-stop from a standing start, without departing from the marked course or hitting the markers en-route. They shall not be timed and shall be characterised by extended use of low gearing and slow speed. Only one vehicle may be on the course at any time. Recovery locations shall be controlled and there shall be start and finish garages.

2.15.1.3 LONG COURSE

This shall be a driving and recovery Stage in which all Competitors compete over similar terrain but not necessarily over the same track. Several competing vehicles may be on the course simultaneously. A Long Course may consist of muddy, rocky, slippery surfaces, steep and difficult terrain which may have controlled self-recovery sections. This type of Stage/s may be judged on distance travelled and may include Individual Skill Stages and Navigation Stages.

2.15.2 GROUP B EVENTS

2.15.2.1 SHORT COURSE

A Short Course is a driving and self-recovery Stage in which all Competitors compete over the same course. Only one vehicle may be on the course at any time. The course may comprise muddy, rocky and slippery surfaces, steep and difficult terrain, and may have controlled and uncontrolled self-recovery sections. The Stage shall be timed. This type of Stage may be judged on distance travelled and may include Individual Skill Stages and Navigation Stages. Both start and finish boxes may be used.

2.15.2.2 OVERLAND COURSE

This shall be a driving and recovery Stage in which all Competitors compete over similar terrain but not necessarily over the same track. Several competing vehicles may be on the course simultaneously. An Overland Course may consist of muddy, rocky, slippery surfaces, steep and difficult terrain which may have controlled self-recovery sections. The Stage/s shall be timed. This type of Stage/s may be judged on distance travelled and may include Individual Skill Stages and Navigation Stages. Start and finish garages or flying finishes may be used.

2.15.3 GROUP C EVENTS

2.15.3.1 SHORT COURSE

A Short Course shall be a driving and self-recovery Stage in which all Competitors compete over the same course. The course may comprise muddy, rocky and slippery surfaces, steep and

difficult terrain, and may have controlled and uncontrolled self-recovery sections. The Stage shall be timed. This type of Stage may be judged on distance travelled and may include Individual Skill Stages. Start and finish garages may be used.

2.15.4 GROUP D EVENTS

2.15.4.1 SHORT COURSE / TUFF TRUCK CHALLENGE - NON TIMED EVENTS

A Short Course shall be a driving and self-recovery Stage in which all Competitors compete over the same course. Only one vehicle may be on the course at any time. The course may comprise muddy, rocky and slippery surfaces, steep and difficult terrain, and may have controlled and uncontrolled self-recovery sections. The Stage shall NOT be timed. Speed is not the determining factor. This type of Stage may be judged on distance travelled and may include Individual Skill Stages. Start and finish garages & point progression markers may be used.

2.16 GENERAL REQUIREMENTS

Vehicles shall comply with the Standing Regulations, Sporting Regulations and the Vehicle Specifications for the relevant Cross Country Four Wheel Drive Event Group, and the Supplementary Regulations for the Event.

2.17 TECHNICAL INSPECTION

Vehicles may be required to be submitted at any time for a thorough technical inspection by an Event Scrutineer.

2.18 PRACTICE

Practice shall not be permitted for Cross Country Four Wheel Drive Events on the site of the Event, unless otherwise advised in the Supplementary Regulations.

The Competition Manager may provide a test area, away from any public or other competitors with marshals to ensure safe testing of vehicle repairs.

2.19 SCRUTINEERING

2.19.1 VEHICLE SCRUTINEERING

Each competing vehicle shall undergo a detailed vehicle safety, modification & equipment examination, in order to obtain a log book.

A log book is mandatory before a vehicle can participate in any event, including any reconnaissance, practise or prologue.

Once a vehicle has been scrutineered and been issued with a log book it is deemed to have passed scrutineering unless it is involved in an incident. It is the driver's responsibility of any vehicle involved in an incident, or which suffers significant mechanical failure, e.g. loss of brakes or steering, to present their vehicle for re-examination before being permitted to take further part in an Event.

Vehicles must be re-scrutineered and log book stamped annually. This must occur prior to any event that is on or after this date.

2.19.2 EVENT SCRUTINEERING

All vehicles shall present at the start of an event for event scrutineering which includes but is not limited to: - Technical Inspection; Rollover Protection Inspection; Recovery & Safety Equipment Inspection; and any special equipment that is required to participate in that Event.

Any vehicle that does not comply with any part of the Event Scrutineering will not be eligible to participate in that Event.

No vehicle shall compete, or continue to compete after any re-examination, without the approval of the Event Scrutineer.

Event Scrutineering shall be outlined in the Supplementary regulations.

2.20 BRIEFING

The Event Organiser shall provide for a briefing of Competitors and Officials before the start of any Event and/or Stage. Such briefing may be verbal or written or both.

2.21 COURSE LAYOUT

The course shall be laid out consistent with the challenging nature of Cross Country Four Wheel Drive Events. Event Organisers must ensure the course is laid out clearly and there is no ambiguity nor should any safety issues be affected by the layout.

2.22 SURFACE

The surface of the course may be varied, as may the terrain, in order to test the Competitor's skill level in a range of off road conditions; e.g. ruts, water crossings, ascents, descents and wet and dry tracks.

2.23 WATER CROSSINGS

The depth and duration of water crossings should be such that a Competitor's ability to complete the Event is not unduly jeopardised. The Event Organiser shall ensure there is a clear and safe passage through any deep water

2.24 STAGE START

The Start shall be clearly marked by two pegs, one on each side of the course. In a timed event if a Competitor "jumps" the start a 10-second penalty shall be applied. Unless the stage regulations nominate a "reverse start" all starts are in a forward direction.

2.25 STAGE FINISH

2.25.1 FINISH BOXES

Finish boxes shall be clearly marked with a finish sign on either of the finish pegs.

The finish box shall be 4 metres long and 4 metres wide, closed with bunting on three sides, leaving the approach side open.

The finish box shall be clearly visible from a distance of 25 metres by Competitors when competing on the course. If the finish box is not visible from this distance a "Pending Finish" sign shall be placed along the course 25 metres before the finish box.

At the finishing box due care must be taken to ensure a safe approach and an overrun exit is clear at all time

Finish boxes shall have adequate spectator and Marshal bunting.

Any overrun of a finish box in a manner deemed dangerous to others by the Stage Manager, Competition Manager or Event Organiser may be grounds for penalty (DNS) or disqualification at the discretion of the Stage Manager, Competition Manager or Event Organiser.

2.25.2 FLYING FINISH

Should several competing vehicles be on a course simultaneously it may be appropriate to have a “flying finish” to minimise potential hazards at the finish point.

The flying finish shall be clearly marked with a finish sign on either of the finish pegs.

The flying finish markers should be clearly visible from a distance of 25 metres by Competitors when competing on the course. If the finish is not visible from this distance a “Pending Finish” sign shall be placed along the course 25 metres before the finish markers.

Sufficient trafficable area shall be allowed beyond the flying finish markers for vehicles to come to a stop safely, clear of following vehicles.

Flying finishes shall have adequate spectator and Marshal bunting.

2.26 DRIVING CONDUCT

A vehicle alone on a section of a course may use the full width of the course. Should a vehicle be approached by a vehicle from the rear that is either achieving more traction at the time, or is consistently faster, the driver of the leading vehicle shall give the following vehicle right of way as soon as possible, pulling to the side and stopping if necessary.

The desire to overtake shall be signalled by the sounding of the horn and/or the flashing of headlamps and/or radio request.

If a vehicle is winching in a safe location and a following vehicle desires to overtake, and can do so safely without crossing the boundaries of the course, or the live winch cable of the other vehicle, they may do so.

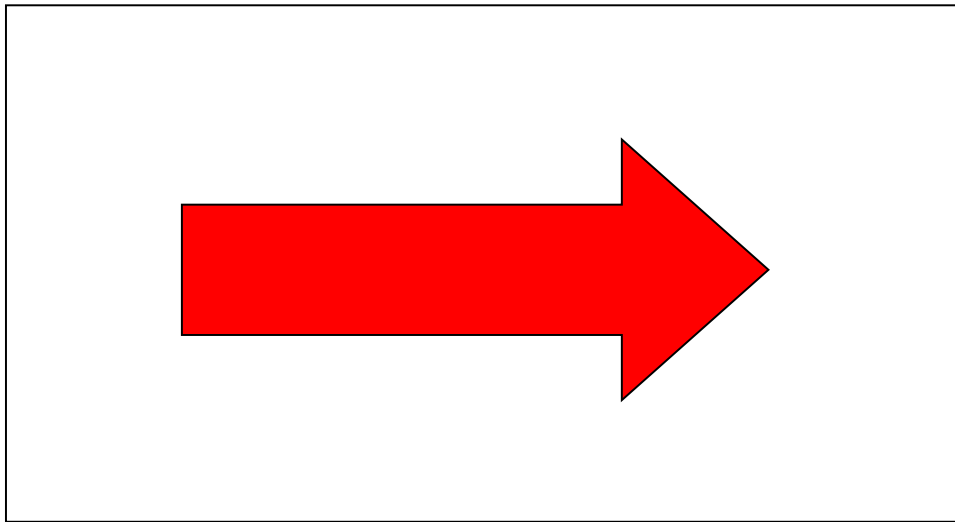
Drivers shall abide by universally accepted practices with regard to minimum impact on the environment.

Competitors shall abide by a Stage Marshal’s instructions, including an instruction to stop competing and exit the course.

2.27 COURSE MARKING

The Event Organiser shall provide a detailed description of each course and advise whether or not it is point-to-point navigation, with or without boundaries, or a restricted course within defined boundaries.

The course shall be clearly defined and arranged in such a way that there is no likelihood of competing vehicles straying from, or of a collision between competing vehicles travelling in opposite directions on adjacent courses or sections of the same course.



Directional Sign (Red arrow on White background)

Examples of all signs used for course marking shall be prominently displayed at the Event location. Directional arrows shall be red, min 250mm long, on a 297mm x 210mm white background. (see above)

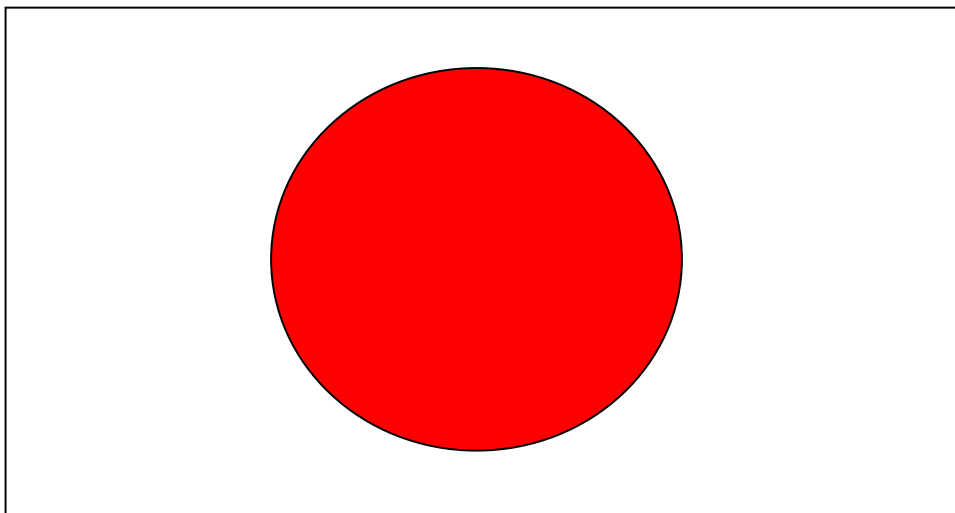
Markers shall be placed where they are least likely to be dislodged by competing vehicles.

Gates used on undefined courses shall be comprised of 2 pieces of bunting/tape tied separately around 2 pegs or 2 fixed items such as a tree trunk. Flexible tree branches should not be used. Gates shall be placed in clear view of approaching Competitors.

Gates shall be driven through in the nominated sequence unless advised otherwise during the stage briefing.

2.28 CAUTION SIGNS AND WARNINGS

Caution signs shall be a red circle, min 200mm diameter, on a 297mm x 210mm white background.

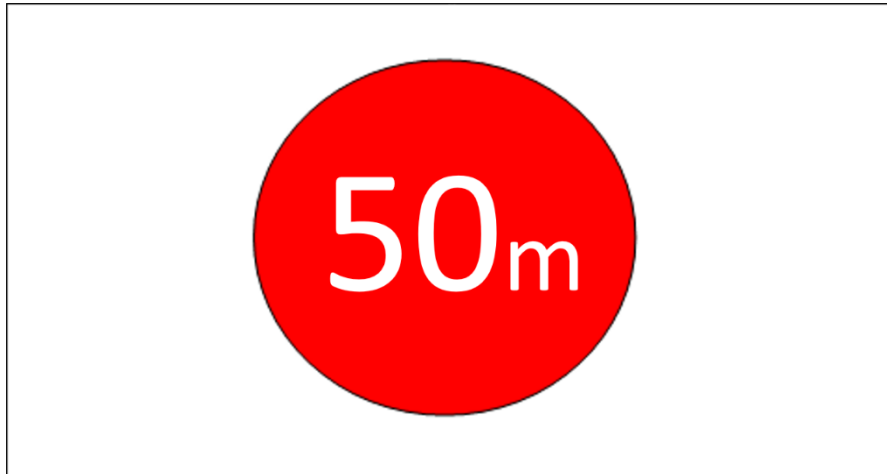


Caution Sign (Red Circle on White background)

Caution signs shall be placed at the subject obstacle on the right hand side of the track 1.5 metres above ground level. Caution Warning signs shall be placed; a) 50 metres before the obstacle and b) 25 metres before the obstacle on the right hand side of the track 1.5 metres above ground level. The absence of any such markings shall not be grounds for protest.

The location of all caution obstacles shall be advised to Crews at the Event and/or Stage briefings.

Caution Warning signs shall be a red disc, min 200mm diameter, on a 297mm x 210mm white background with the relevant distance shown in white on the red circle.



Caution Warning Sign (50 metre sign shown)

2.29 ABANDONMENT OR CANCELLATION OF A STAGE OR EVENT

An Event or a Stage of an Event may be abandoned or cancelled after commencement at the discretion of the Event Organiser. The reasons for the abandonment or cancellation shall be communicated to the Competitors as soon as practical after the abandonment or cancellation at a briefing called for that purpose.

Should twenty percent or more of the qualified field of Competitors have started the Event or Stage subject to the abandonment or cancellation, and further provided that the field started in ranked order for that Event or Stage, then the points earned by the Competitors who started the Event or Stage shall count toward the Event result.

A qualified field shall include those Competitors whose vehicle had passed Event Scrutineering and who subsequently had not withdrawn or been disqualified, and whose vehicle was readied and able to start the Event or Stage.

The balance of the qualified field of Competitors who did not start the Event or Stage before its abandonment or cancellation shall be awarded the same score as the lowest scoring Competitor who started the Event or Stage, except that any penalties incurred by that Competitor shall not be applied to those Competitors who did not start.

Should less than twenty percent of the qualified field start an abandoned or cancelled Event or Stage of an Event, or should the qualified field not start in ranked order for that Event or Stage, then points earned by any Competitor who started that Event or Stage shall not count toward the Event result.

2.30 RECOVERY SITUATIONS

Recovery situations should be incorporated during Stages to emulate common circumstances normally found whilst four wheel driving. Recovery sections should be achievable by Competitors within the allocated time.

2.31 INDIVIDUAL SKILL STAGES

Individual Skill Stages may involve first aid, navigation, survival, mechanical, or other tasks. If special equipment is required, the details of such equipment shall be included in the Supplementary Regulations. All reference material shall be available to all Competitors prior to the Event. Competitors shall be notified of all reference material used to set the Individual Skill Stages.

2.32 START ORDER

Vehicles shall start in the order prescribed by the Event Organiser. Options for determining start order may include using the results from the last time the same Stage was completed, by ballot, or by a Prologue. Whenever practical the starting order for subsequent Stages shall be selected by the latest available progressive Event placings. Consecutively run Stages may utilise the same starting order until updated results can be calculated. Once the Event commences Crews with close results shall compete consecutively on the same Stage.

Should any Competitor not attempt or complete any Stage due to accident or mechanical malfunction, the Event Organiser may allocate a starting position.

The driver shall align the forward-most part of the vehicle with the starting pegs at the start of any Stage, or as guided by the starting Marshal.

The driver and co-driver shall each be seated in the vehicle with occupant restraints correctly fastened at the start and finish of each Stage unless otherwise specified by the Competition Manager.

2.33 STARTERS ORDERS

Drivers shall be under starting Marshal's orders upon arrival at the start line. Any Crew failing to proceed to the start at the direction of an Official may forfeit the run, or may be deemed by the Starting Marshal to have started at his or her designated start time.

2.34 STAGE INSTRUCTIONS

Stage Instructions shall include start intervals, DNF time, course length and features, location of cautions, special instructions or requirements.

Stage Instructions shall be read to the Competitors from a prepared Stage Instruction sheet by the starting Marshal, or included in Stage route notes or guidebook.

There shall be no variation to the Stage Instructions without the Competition Manager's permission.

2.35 COURSE CHANGES

If the course has been changed the Competitors shall be notified prior to any Competitor commencing that Stage. Should exceptional circumstances indicate that a course change is necessary after some Competitors have completed a course, only those changes that can be clearly demonstrated to present no advantage nor disadvantage for following Competitors may be implemented. Otherwise the course shall be abandoned.

Should the course become impassable or unsafe that Stage shall be cancelled except that an alternative course may be selected as an alternative Stage with a new identification number and all vehicles shall then become eligible to start that alternative Stage.

2.36 ADDITIONAL STAGES

Should it become likely that the Event program may be completed prematurely, and there is reasonable time available, additional Stages may be introduced. Should it become likely that the Event may not finish on time some Stages may be cancelled partially or completely with the authority of the Competition Manager. Adequate notification of any such change, according to the nature of the Event, shall be given and any additional Stage shall have a new identification number and all vehicles shall then become eligible to start that additional Stage.

2.37 PERMISSION TO START

No Stage may be commenced without the permission of the Competition Manager.

2.38 VEHICLE RECOVERY CONDUCT

Competitors shall abide by internationally accepted practices during recovery situations described herein. Note that the use of the winching term “cable” within this manual refers to synthetic winch rope.

2.39 UNACCEPTABLE RECOVERY PRACTICE

All winching shall cease until the following unacceptable practices are corrected:

- a) Failure to use a tree protector of sufficient length to completely encircle a tree or stump being used as an anchor point.
- b) Failure to maintain a cable dampener where required while winching;
- c) The location for dampener for synthetic rope/strap combination shall be at connecting metal objects (e.g. bow shackle, snatch block etc)
- d) Crew members shall not control a winch from outside the vehicle when the cable is live unless a remote controller is used and the crew member is at least 1 metre away from the vehicle.

2.40 WINCH LINE RETRIEVAL

Crews shall stand clear of a winch line being retrieved and shall keep all body parts clear of the fairlead when rewinding the winch line. Marshals will repeatedly warn crews if they feel that competitors are not clear of the fairlead.

Hooks or loops in the end of the cable shall not be left dangling such that the hook or loop can touch the ground. If the hook or loop is touching the ground at the finish the marshal will award a penalty for “equipment not secured”.

2.41 ALTERNATING DRIVER

Alternating between Driver and Co-driver for any Stage of the Event shall be permitted, unless it is specified otherwise by the Event Organiser or Supplementary Regulations.

2.42 TIME ALLOWED FOR STAGE COMPLETION

The Event Organiser shall exercise care to ensure that adequate time exists for the completion of each Stage of the Event.

2.43 PARC FERME (CLOSED CARPARK)

A parc ferme or “closed carpark” is used on some events to ensure that all competitors have an equal time to work on their vehicles. During a declared parc ferme, competitors are not permitted to work on their vehicles. The Stage Manager will determine a location for parc ferme and all competitors directed to that location at the end of the stage. During a parc ferme restriction competitors may access their vehicles for food or communications etc. however no work on vehicles shall be undertaken. Once all competitors have completed the stage the parc ferme is declared off and work may be undertaken on vehicles

Should a parc ferme be used at an event, competitors will be advised when this condition is in place.

2.44 RESULTS

The Event Organiser shall forward results to each Competitor as soon as reasonably possible after the Event.

2.45 CANCELLATION, ABANDONMENT AND ALTERATION

The Event Organiser may abandon an Event or Stage if in his or her opinion the number of entries is insufficient, or if conditions constitute force majeure.

If the Event or Stage is abandoned when part-run, the provisions of Rule 2.29 shall apply

2.46 PRESENTATION OF AWARDS

Following each event, the Event Organiser shall conduct a presentation ceremony to ensure that the public and others are able to properly identify and honour the winners.

2.47 MEDIA CONFERENCE

Award-winning Crews may be required to attend a media conference, which shall take place as close as practicable to the presentation area.

Crews are reminded of their obligations to the Event Organiser and sponsors, and shall present themselves accordingly.

2.48 FIRST AID

Unless specified otherwise in Supplementary Rules, competing vehicles shall carry an adequate outdoors first aid kit in or on the car at all times and it must be readily accessible.

2.49 FIRE EQUIPMENT

Vehicles shall carry at least one 1.5kg, or two 0.9kg dry powder fire extinguisher(s) inside the cabin placed in a non-vertical mounting position, in a readily accessible location for both members of the Crew. In addition, a 1.5kg, or two 0.9kg dry powder extinguisher(s) shall be mounted exterior to the cabin, where possible, and be accessible by a third party standing on the ground beside the vehicle. In enclosed vehicles this must be mounted in the rear as accessible as possible with min. 100mm Diameter Red letter “E” in a circle sticker (white background) applied on the exterior of the vehicle at the extinguisher's location. Extinguishers fitted in the cabin shall be below shoulder height. Mounting brackets shall be steel. Plastic mounting brackets shall not be permitted.

Fire extinguishers shall be within the manufacturers specified expiry date and be removed and powder unsettled annually.

2.50 SPECTATORS

Should spectators be admitted the Event Organiser shall provide safe viewing facilities. Spectators should be prevented from approaching a competition course at each spectator access location by a barrier of an identifiable material located such that spectators remain a safe distance from the course. Each spectator point shall be manned by clearly identified spectator Marshals who shall work under the direction of a Senior Marshal. Areas in which spectators may not stand shall be clearly identified.

2.51 COMPETITORS PIT AREA AND CAMPING GROUNDS

A designated pit area will be fenced or bunted and general public or spectators will not be granted entry to this area. Where possible competitor camping areas and pit areas should be separate, where this is not practical then designated working areas within the pits should be provided to allow safe vehicle repairs away from non essential people.

There shall be a designated roadway in and out of the pit area, away from spectators or public. Vehicle movements within the pit area must be limited to entering or leaving via the nearest entry/exit via the most direct route adhering to the non competitive speed limit.

Events sites at which camping is permitted shall, where practical, have separate camping facilities for Competitors and spectators. The Supplementary Regulations shall specify the location of camping facilities provided for Competitors and spectators.

2.52 ADVERTISING AND DECALS

Each vehicle may display Crew sponsorship material provided it is not, in the sole opinion of the Event Organiser, offensive and does not intrude into the space allocated for Event sponsorship/advertising.

Event sponsorship/advertising shall be limited to:

- An area of approximately 600x600 for event sponsor and vehicle numbers.
- An area of 100mm height along the top or bottom of the windscreen or leading edge of the roof.

Additional Event sponsor decals typically small in size may be placed at any suitable location on the body of the vehicle but shall be clearly visible.

MANUAL OF 4WD MOTOR SPORT
SECTION 3 – SPORTING REGULATIONS



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3 SPORTING REGULATIONS

3.1 TIMING

Timing shall be to the elapsed hundredth of a second, expressed in hours, minutes, seconds, tenths of a second and hundredths of a second using the 24-hour system.

e.g.: 01 Hr 25 Min 15.56 Sec

Timing shall start on Go, after a countdown of five, four, three, two, one. The vehicle shall remain stationary until timing starts.

Once the countdown has commenced, should the competitor move in any direction ahead of "Go" a 10 second penalty (Jumped Start) shall be applied.

Should the infringement be deemed by the Competition Manager to be a deliberate attempt to gain unfair advantage the penalty may be elevated to a DNF.

A secondary timing device shall be used as a reserve for the primary timing device. The time recorded by the secondary timing device shall be disregarded unless the primary timing device fails to function.

3.2 STOPPED FINISH

Timing of a "Stopped Finish" Stage shall cease when at least one front hub of the competing vehicle passes the entry pegs to the "Finish Box" and the vehicle comes to a complete stop prior to the end of the enclosure. The crew shall be seated with seat belts or harness correctly fastened, helmet on (in group B and C events or where nominated), the doors closed and window nets in place.

The Stage Manager shall be the judge of fact with regard to time and direction of entry into the finish garage.

Competitors shall not move from the finish garage until directed by a Marshal.

3.3 FLYING FINISH

The timing of a flying finish Stage shall cease when the forward-most part of the vehicle crosses the finish line.

3.4 OBSTRUCTION TIME ALLOWANCE

The Competition Manager or Stage Manager may award obstruction time to any Competitor who stops during a Stage due to external factors not intended to be part of the Event.

3.5 COMPASSIONATE TIME ALLOWANCE

The Competition Manager or Stage Manager may award compassionate time to any Competitor who stops during an Event to render assistance where bodily harm might reasonably be expected to have occurred, or to be imminent.

3.6 STAGE RULES AND REGULATIONS

3.6.1 OCCUPANT PROTECTION

In Group A Events seat belts or harnesses shall be properly fitted, correctly fastened and adjusted during all Stages whilst the vehicle is in motion. No member of the Crew shall ride on the outside of a competing vehicle and no member of the Crew inside the competing vehicle shall have any limbs outside the vehicle whilst the vehicle is in motion.

In Group B and Group C Events harnesses shall be properly fitted, correctly fastened and adjusted during all Stages whilst the vehicle is in motion. No member of the Crew shall ride on the outside of a competing vehicle and no member of the Crew inside the competing vehicle shall have any limbs outside the vehicle whilst the vehicle is in motion.

Unless required by supplementary regulations, window nets are not compulsory in Group A Events.

Whilst competing in Group B Group C and Group D Events window nets are required to be fitted to the drivers' and passengers' window aperture. The nets must cover at least 70% of the opening and be readily opened from the top or bottom, from inside and outside the vehicle. The nets may be of the "string" variety with mesh size no greater than 50mm x 50mm.

Window nets shall be properly fitted during all Stages whilst a person is present in that seating position and the vehicle is in motion. Window nets may only be removed when crossing deep water. A 10 point penalty shall be incurred for vehicles found with window nets not in place during competition.

3.6.2 CRASH HELMETS

Unless required by supplementary regulations, wearing of crash helmets is not compulsory in non-vehicle based Events or Stages.

Occupants shall wear Australian Standard Certified crash helmets to AS 1698 whilst competing in Group A, Group B, Group C and Group D Events. The helmets must fit correctly and be secured with the chin straps.

3.6.3 COURSE MARKERS

The course of a Stage may be marked by pegs or bunting or be stated as a formed track. Penalties shall be deducted for hitting pegs or infringing the course boundaries specified in the Event Supplementary Regulations. In the Event of a Stage consisting of gates on an undefined course, all gates shall be negotiated correctly and in the specified sequence.

3.6.4 SUPPLEMENTARY RULES AND REGULATIONS

The Event Organiser may add supplementary rules and regulations and/or further Regulations at any time during the running of the Event. The Event Organiser may decide on matters not covered in this set of rules and regulations and allocate penalty points at their discretion.

The Event Supplementary Regulations shall specify whether telephones or radios may or may not be used to share information during Navigation exercises.

Event Organisers shall submit their supplementary rules and regulation to the CCDA committee for review not less than 60 days before the intended start date of the event. Supplementary rules and regulations shall not be implemented without consultation with the CCDA or their nominated representative.

3.7 DNS (DID NOT START)

A Competitor who fails to start 60 seconds after the timing has commenced for a Stage shall be deemed DNS. Also refer to Rule 2.33, deemed by the starting Marshal to have started.

A Competitor not ready to start in their allocated order at the time the previous Competitor leaves the starting line, shall be deemed DNS. Also refer to Rule 2.33, deemed by the starting Marshal to have started.

Competitors deemed DNS in a Stage, shall receive zero points in that Stage.

A Competitor shall be deemed DNS in a Stage if they have not passed a qualifying marker which is commonly the first obstacle.

3.8 DNF (DID NOT FINISH)

A time shall be allotted for the completion of each Stage. A Competitor shall be deemed DNF if they have not completed the Stage in the allotted time and shall immediately remove their vehicle from the course to allow other vehicles to start.

A Competitor may choose to DNF due to a mechanical fault. The Stage Marshals shall then remove the vehicle from the course in order to start the next Competitor as soon as possible.

A Competitor shall be deemed DNF if they receive any outside assistance during the running of a Stage.

Competitor shall be deemed DNF for that Stage if all four wheels of a competing vehicle cross a clearly defined boundary of the course or does not carry out the instructions of the Starting Marshal in respect to the sequence of activities that the stage has been designed to provide. The competitor shall immediately remove their vehicle from the course to allow other vehicles to start.

A Competitor deemed DNF in a Stage shall receive 20 Stage points provided they have passed a qualifying marker which is commonly the first obstacle.

3.9 DNF (DID NOT FINISH) - GROUP D EVENTS ONLY.

This rule is applied for Group D events where speed is not the determining factor. A DNF Time limit is given for each stage, as some stages vary, the allotted DNF time is noted in the stage specific rules given to each competitor at the beginning of the stage.

1. Competitor's time is not recorded or used to determine a competitor's place in the competition.
2. The DNF time limit for completing a stage is applied equally to each competitor.
3. A competitor is not to be rewarded or given any advantage in the event that they finish a stage within the DNF time limit
4. If a competitor takes longer than the allotted DNF time limit to finish a stage, any points accumulated up until the point of disqualification will still count towards a competitor's final score.
5. Any additional points incurred as a result of completing a "bonus line" are not worth more if a competitor completes a stage within the allotted DNF time limit.

3.10 WINCHING/ RECOVERY RULES AND REGULATIONS

3.10.1 RECOVERY PENALTIES WITHOUT WARNING

Breaches of the following practices shall attract a penalty. Marshals shall advise the Crew of the penalty when it occurs either verbally or by using a horn, whistle or flag:

Crews shall clear bystanders in the vicinity of a recovery situation prior to commencement of winching. Calling "clear, winching" or similar shall be deemed an acceptable means of clearing the area. In the event that the anchor point is relocated then the winching procedure is deemed to recommence and calling "clear, winching" or similar shall again be necessary.

Winching and driving shall cease when re-positioning or re-attaching the cable dampener.

The cable and/or anchor shall not be handled while the cable is under tension.

Gloves on both hands or equivalent, with covered fingers, shall be used when handling a winch cable.

Crew members shall stand clear of the cable and must not stand directly in front of the vehicle during winching. Winching and driving shall cease when a Crew member approaches the vehicle to inspect the cable on the drum, pack rocks, adjust dampener, or any other reason.

Crew members must not at any time handle a cable when the vehicle is being driven in any direction. This includes the cable hook or dampener attached to the recovery rig.

A cable attached to a winching point is defined as "live" and Crew members shall not cross over or pass under a live cable. The cable attached to a ground anchor used in a recovery situation is deemed to be "live" when the ground anchor is placed in position and under "tension" when there is enough grip to move the vehicle.

A crew member, when returning to the vehicle or packing up the winch, must remain outside the "loop" of cable formed between the winch drum and the winch hook storage point on the vehicle once the hook is connected to the storage point. Being inside the loop at any time will incur a penalty.

The action of Crew members shall not cause any risk to any person.

3.10.2 RECOVERY PENALTIES AFTER WARNING

Breaches of the following practices shall attract a penalty on their second and subsequent occurrence. Marshals shall advise the Crew of the first infringement when it occurs either verbally or by using a horn, whistle or flag:

3.10.3 INCORRECT USE OF A CABLE DAMPENER AFTER ONE WARNING

- When using a snatch block to change the direction of pull a cable dampener shall be securely attached to the snatch block.
- Where a winch extension strap or cable is used, a dampener shall be securely attached to any metal equipment (e.g. Bow shackles) when in use.
- Should the cable dampener become detached, winching must cease until it is re-secured metal recovery equipment (e.g. shackle / snatch block)
- Cable dampeners may not be folded more than once. The cable dampener shall not be rolled. Winching shall cease until any or all of these issues are rectified.
- The dampener may be removed if the vehicle is less than a nominal two metres from the anchor point.

3.10.4 INCORRECT USE OF A GROUND ANCHOR AFTER ONE WARNING:

Crew members shall consider a ground anchor and cable as part of an active winching system once the ground anchor is in position and has sufficient grip to move the vehicle and the crew shall remain clear of both the ground anchor and cable.

Crew members may "ride" the ground anchor but must come off the anchor once the vehicle is moving. A penalty will be awarded, after warning, if the crew fails to stay clear of the ground anchor and cable once it is "live" It is recognised that the vehicle may stop and the crew member may need to "ride" the anchor again to re-secure the anchor. A Marshal advising the crew that the vehicle is moving is not considered to be a "Warning".

3.10.5 EXCESSIVE WHEELSPIN:

Where there is no forward progress in the one location and the crew spins the wheels excessively they shall not be allowed to continue if track damage may occur or if directed to cease by a Marshal. Failing to abide by the Marshals instructions shall incur a penalty.

3.10.6 SAFE RECOVERY SETUP

Crew members must not at any time attach a winch hook or cable to any part of their person or clothing.

3.10.7 CABLE REPAIRS

Should a winch cable break during a Stage, temporary repairs may be carried out to complete that stage. The damaged cable shall then be replaced or reinstated to good condition in accordance with the scrutineering standard established for the Event.

3.10.8 PENALTIES

3.10.8.1 LEVEL OF PENALTIES

The penalty level of 10 points represents 10% of the maximum 100 placing points available to be earned in a Stage. If the maximum placing points available to be earned in a Stage varies then the penalty level shall be varied accordingly.

e.g.: If the maximum placing points available to be earned in a Stage is 200 then each penalty shall be set at 20 points.

3.10.8.2 COURSE PENALTIES

A 10 point penalty shall be incurred without warning for each of the following breaches:

- Breaking a peg, knocking a peg over more than 45° from vertical and/or a peg lying on or pulled out of the ground.

- Breaking any bunting along the course or garage.

- Lost or unsecured equipment at the completion of the Stage.

- Excessive wheelspin causing track damage or after being directed to cease;

- Action causing risk to any person

3.10.8.3 WINCHING PENALTIES

A 10 point penalty shall be incurred **without** warning for each of the following breaches:

- Failing to call clear before commencing to winch;

- Failing to cease winching and driving when repositioning dampener;

- Handling cable under tension;

- Failing to wear gloves when handling a cable;

- Standing in front of vehicle being winched;

- Crossing a live cable;

- Causing risk to any person;

- Handling a cable when vehicle is being driven;

Lost or unsecured equipment at the completion of the Stage;
Cable dampener not placed in correct position during winching;
Crew member inside the cable 'loop'.

A 10 point penalty shall be incurred **after** warning for each of the following breaches:

Incorrect use of a ground anchor;

3.10.8.4 CODE OF CONDUCT PENALTY

Physical abuse of any kind shall result in instant disqualification from the event.

A 100 point penalty shall be incurred on the first occasion of verbal or physical abuse of any event official.

Subsequent abuse of any event official will result in disqualification from the event.

3.10.8.5 OCCUPANT RESTRAINT AND CRASH HELMET PENALTIES

Seat belts/harnesses as specified in Rule 4.1.3 shall be properly fitted to each vehicle for each occupant and shall be correctly fastened and adjusted at all times that the vehicle is in motion.

If an official detects that the seat belts/harnesses/helmet chin straps are not correctly adjusted the official shall stop the vehicle and request that they be adjusted correctly.

On the first occasion that any occupant of a vehicle is detected not wearing a helmet (in group A, B and C events or where nominated), or their seatbelt/harnesses are not correctly fastened while the vehicle is in motion that Crew shall be disqualified from the Stage during which the infringement is detected, and no points shall be awarded for that Stage.

On the next occasion that any occupant of the same vehicle is detected not wearing a seatbelt/harness or helmet while the vehicle is in motion that Crew shall be disqualified from the Event.

If an official detects that a vehicle does not have window nets in place during competition the official shall stop the vehicle and request that they be fitted correctly. A 10 point penalty will apply for each occurrence.

3.11 SCORING

The scoring system shall be such that all Competitors shall have the same opportunity to achieve the same points for each Stage, and the same margins between placings in each Stage, for equal effort, recognising that there are a variety of skills required to navigate a four wheel drive vehicle over a variety of terrains within a defined period. Each class should be scored individually at the discretion of the event organiser.

The scoring system shall also reward Competitors, who continue to attempt any Stage, by allocating DNF points for every Stage.

3.11.1 POINTS ALLOCATION TABLE

1 st	100	11 th	66	21 st	55	31 st	45	41 st	35
2 nd	95	12 th	64	22 nd	54	32 nd	44	42 nd	34
3 rd	91	13 th	63	23 rd	53	33 rd	43	43 rd	33
4 th	87	14 th	62	24 th	52	34 th	42	44 th	32
5 th	83	15 th	61	25 th	51	35 th	41	45 th	31
6 th	79	16 th	60	26 th	50	36 th	40	46 th	30
7 th	76	17 th	59	27 th	49	37 th	39	47 th	29
8 th	73	18 th	58	18 th	48	38 th	38	48 th	28
9 th	70	19 th	57	29 th	47	39 th	37	49 th	27
10 th	68	20 th	56	30 th	46	40 th	36	50 th	26

A Competitor finishing within the DNF time but placed after 50th shall receive 25 points.

A Competitor deemed DNF in any Stage shall receive 20 points, subject to the provisions of Rule 3.9. Penalty points may still be applied.

A Competitor shall receive a minimum of 20 points before any penalties points are applied.

A Competitor shall not receive less than zero points if penalties exceed score gained.

3.12 STAGE PLACINGS

Stage Placing's shall be in the order of points achieved, after deducting any penalty points that may have been incurred, the highest number of points being placed first. The application of penalties may result in a Stage being won by a Competitor who scores less than the maximum available points.

e.g.: if the maximum available points is 100, and the highest placegetter incurred one 10 point penalty during the Stage, they would finish the Stage with 90 points, placing them in equal third place and the second highest placegetter would be the Stage winner with 95 points.

3.12.1 TIMED STAGES (NOT APPLICABLE IN GROUP D EVENTS)

Competitors who complete a timed Stage within the DNF time shall be allocated points according to their elapsed time from fastest to slowest in accordance with the Points Allocation Table. Stage placings shall be determined after any penalties incurred have been deducted.

Should two or more Competitors finish a Stage with the same elapsed time, they shall be awarded equal points according to the Points Allocation Table. The Competitor with the next fastest time shall be allocated points according to their own elapsed time in that Stage. The placing(s) left vacant due to any tie shall not be allocated.

e.g.: if two Competitors finish with equal third fastest time and are placed equal 3rd place, the next finishing position is 5th place. i.e.; both Competitors receive the points for third place according to the Points Allocation Table and fourth place is not allocated.

If no Competitor reaches the finish line, or completes the Stage in the allotted time, the Event Organiser may allocate points in the order of who advanced the furthest into the Stage.

3.12.2 TIMED NAVIGATION STAGES (NOT APPLICABLE IN GROUP D EVENTS)

A Competitor who successfully completes the most tasks or correctly achieves the highest number of navigation markers, or both, within the DNF time shall be allocated points from the Points Allocation Table in accordance with the following principles:

Competitors shall be placed in order of merit by highest to lowest number of tasks completed within DNF time.

Any Competitors with equal tasks completed, shall be separated by the lowest time achieved, thus gaining the higher placing.

e.g.: The Stage might require 80 navigation markers to be found within 3 hours. If the greatest achievement is by one Competitor who returns with 60 markers, then that Competitor receives 100 points. If then the next best achievement is by 5 other Competitors who each return with 58 markers the Competitor with the lowest time out of those 5 receives 95 points. The second lowest timed Competitor of the 5 receives 90 points, and so on.

3.12.3 COMBINATION TIMED AND DISTANCE STAGES (NOT APPLICABLE IN GROUP D EVENTS)

Competitors who complete a Stage judged on both distance and time shall be allocated points based on the markers passed by the forward-most wheel hub of the vehicle and shall also be ranked based on time taken to reach the Finish within the DNF time. The Stage placing shall be decided by ranking Competitors first by greater distance achieved, then time taken should two or more Competitors achieve the same distance.

Competitors who do not reach the Finish shall still be considered finishers and placed according to the distance travelled by the forward most wheel hub of the vehicle within the allocated DNF time.

e.g.: Two Competitors pass all the markers required along the course and make it to the finish box within the DNF time. The Competitor with the lowest time is placed first and the other, placed second. All the other Competitors reach various distances along the course and do not reach the finish box before the DNF time. These Competitors are placed according to the furthest distanced achieved gaining the higher placing. Points are then allocated, according to the Points Allocation Table.

Should two or more Competitors finishing a Stage with the same completed tasks and elapsed time, they shall be awarded equal points in the Stage, less penalties incurred. The next fastest time shall be placed according to their own finishing position in that Stage. The placing(s) left vacant due to the tie shall not be allocated.

3.12.4 UNTIMED NAVIGATION OR THEORY STAGES

A Competitor who answers the most questions correctly within the DNF time and/or correctly enter all checkpoints and/or locate specific markers correctly and/or complete special tasks correctly shall be ranked in terms of total points achieved and awarded points according to their placing on the points allocation table.

3.12.5 UNTIMED DISTANCE STAGES

A Competitor who travels the greatest distance over arduous terrain within the DNF time shall be ranked by one of the following methods:

Placing decided based on the distance travelled by the forward most wheel hub of the vehicle once forward momentum has stopped.

Placing decided based on the distance travelled in a set DNF time.

Placings shall be awarded in order of the furthest distance travelled to the least distance travelled and allocated points according to the Points Allocation Table.

3.12.6 TEAMS STAGES

If Teams Stages are included, the Teams Stage results shall be calculated separately from individual Competitor Stage results, with separate Teams trophies awarded.

3.13 TEAMS EVENTS

Event Supplementary Regulations shall describe the scoring system for Teams Events

3.14 EVENT PLACINGS

Should two or more Competitors finish the Event on the same points, Event placing shall be determined by reference to each Competitor's placing in each Stage conducted.

Prologue results shall be added to, and included in, overall results.

To determine the Event placing, the Competitor with the greater number of first places in Stages shall be declared the higher placegetter. If this fails to break the tie then the Competitor with the greater number of second places shall be declared the higher placegetter. Failing that, reference shall be made to the greater number of third places and so on until a placegetter is determined.

3.15 PERMITTED SERVICING

Crews shall be self-sufficient throughout Cross Country Four Wheel Drive Events.

Allowable servicing of a Competitor's vehicle during a Cross Country Four Wheel Drive Event shall be defined in the Supplementary Regulations. Limitations on servicing or outside assistance may be altered to suit the event and will be specified in the Supplementary Regulations for such an event shall be

Carried out only by Crew members (including pit crew) of a vehicle that started in the Event, whether it is the vehicle in question or another Competitor's vehicle.

In the form of assistance by an official recovery unit from a Stage and takes the form of:

removing the vehicle from the Stage,

provision of tools, parts and/or consumables provided that once clear of the Stage the tools and parts are returned to the official recovery Crew,

provision of water and lubricants to clear the vehicle from the Stage.

Crews may attempt to make repairs to finish a stage within time provided they do not procure parts, tools or any other form of outside assistance from any non-competitor or competitor not currently competing on the same stage.

Should the Supplementary Regulations nominate specific service times and designated service areas the permitted servicing shall be confined to such times and areas.

Mobile support or service vehicles shall not be permitted to accompany or attend Crews or Competitor's vehicles unless nominated in the Supplementary Regulations for that Event.

3.16 RESULTS CLARIFICATION

Competitors may seek clarification of their score for any Stage from the Competition Manager. If there is a dispute then the Protest procedures in Rule 3.17 shall be followed.

3.17 PROTESTS

Protests shall be handed in writing to the Competition Manager/Chief Steward within two hours of receiving the score of the Stage being protested. A point deposit of 300 points (or 10% of the total available points – whichever is greater) shall be required to lodge a protest. A cash deposit may also be required. The deposit(s) may be forfeited if the protest is dismissed. Protest requirements shall be explained in the supplementary regulations of the event.

A judiciary panel shall be appointed at the start of the Event consisting of:

Event Organiser

Competition Manager

Rules Adjudicator

Relevant Stage Manager

Competitors' Representative

The Chief Scrutineer if the protest is an alleged vehicle specification breach.

The panel shall deliberate on any formal protest and a majority agreement of the panel shall be required for the protest to be upheld.

Should a member of the panel be considered to have a conflict of interest he or she will abstain from voting and be bound by the decision of the remaining members.

Should there be perceived prejudice between the Competitor and a member of the panel that panel member may absent themselves from the panel, or the Competitor may require that member to be removed from the panel. In this circumstance a minimum of 25% of the Competitors shall elect a replacement panel member from the remaining available Stage Managers.

MANUAL OF 4WD MOTOR SPORT
SECTION 4 – VEHICLE SPECIFICATIONS



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4 VEHICLE SPECIFICATIONS

4.1 ALL CLASSES

4.1.1 OVERALL RULING

Vehicle modifications not expressly authorised by these Vehicle Specifications shall be prohibited. The following specifications for this section "All Classes" are the minimum requirements for, and shall apply to, all classes of vehicles.

4.1.2 VEHICLE REGISTRATION

Except for Open & Challenge class vehicles, all vehicles participating in the Event shall be legally registered as a motor vehicle in their State, Territory or Country of origin for use on public roads and shall be maintained in a roadworthy condition.

4.1.3 OCCUPANT PROTECTION

Vehicles competing in Group A Events shall be fitted with lap/sash type seat belts for all occupants with belting that is not frayed or damaged and all mountings shall be secure in accordance with the original vehicle specifications.

Vehicles competing in Group B Events shall be fitted with original vehicle seat belts that is not frayed or damaged and all mountings shall be secure in accordance with the original vehicle specifications.

Vehicles competing in Group C Events shall be fitted with a minimum four point racing harness for all occupants fitted with harness belting that is not frayed or damaged and all mountings shall be secure in accordance with the harness manufacturer's instructions.

Vehicles fitted with 4 or 5 point harnesses must use seats that maintain the correct position of the harness shoulder straps

Unless required by supplementary regulations, window nets are not compulsory in Group A Events. Whilst competing in Group B and Group C Events window nets are required to be fitted to the drivers' and passengers' window aperture. The nets must cover at least 70% of the opening and be readily opened from the bottom or top, from inside and outside the vehicle. The nets may be of the "string" variety with mesh size no greater than 50mm x 50mm.

The location of the net is optional and it may be fitted to the window frame so as not to inhibit egress.

Station Wagon short wheelbase (SWB) and long wheelbase (LWB) type vehicles shall be fitted with rigid cargo safety barriers securely mounted behind the passenger seats. Any equipment, parts and tools carried forward of the cargo safety barrier shall be securely fixed to prevent movement or dislodgment during competitive or transport stages of an Event.

Cargo safety barriers shall not be fabricated from webbing, strapping or any other flexible material. Soft-top, FRP, plastic or open top vehicles, shall have roof protection installed to protect the Crew from intrusions. Roof protection shall consist of a 3mm minimum thickness aluminium sheet or 2mm minimum thickness steel plate or high impact plastic roof panel extending the full width of the vehicle from the windscreen frame to the rollover protection and shall terminate behind the heads of the Crew.

Sunroofs are not permitted on any vehicle competing in timed Events.

Vehicles competing in timed events other than Group A & B events shall be fitted with six point roll-over protection. Refer to Section 5 of this manual. Rollover protection is not compulsory for vehicles competing in Group A events however it is highly recommended.

The minimum design for any rollover protection fitted for Group B events shall be Type 2 (CAMS Manual Schedule J) half cage except that the diagonal brace may be removable, including back stays.

All vehicles shall have a seatbelt cutter secured inside the cabin of the vehicle accessible by both driver and navigator.

4.1.4 BAR WORK

Bars may be added to the front of vehicles for the mounting of recovery points, winch, lights and aerials. Bars may be added to the sides and rear of vehicles for added protection. All bar work is to be capped with no sharp edges.

4.1.5 RECOVERY POINTS

Recovery points shall be mounted at the front and rear of the vehicle chassis with at least two of 12mm or four of 10mm grade 5 or M8.8 bolts or sufficiently welded to the chassis. Recovery points shall be painted red or yellow and shall be capable of carrying a load at least twice the vehicle mass. Eyebolts are not considered as satisfactory recovery points.

Monocoque frame vehicles shall have each recovery point mounted such that the load is applied evenly to the vehicle frame, or have recovery points linked to achieve even load application.

Roof racks/bars where fitted shall be securely fastened. Loads carried on roof racks/bars shall not exceed 70kgs, including the mass of the rack, and no fuel or gas shall be carried thereon.

4.1.6 ACCESSORIES

UHF (477mhz) operational radios shall be fitted to all vehicles.

Portable radios with optional headsets may be used during competition Stages.

Power winches shall be fitted if the Supplementary Regulations specify they are required.

Recovery equipment shall be carried if the Supplementary Regulations specify that such equipment is required and shall be in first class condition and rated at least to be greater than the capacity of the winch and double the weight of the vehicle, Vehicles weighing more than 2000kg shall require the higher specification of the respective equipment items listed below:

- One snatch strap: 60mm width rated 8000kg

- One snatch block: 7000kg

- Two suitable "D", bow or "soft" shackles, 3.25t or 4.75t

- One winch extension strap, 75mm width rated 8000kg or Synthetic Rope 10mm, 8500kg

- One tree trunk protector: 75mm width rated 12000kg. Two tree protectors may be required.

- At least one cable dampener measuring at least 900 x 500mm and weighing at least 1kg when dry.

- Fire extinguishers as required. Refer to Rule 2.49.

- Jack and jacking plate with capacity to lift the vehicle to change a wheel.

- Tools and spares as required to maintain the vehicle.

- Two pairs of winching gloves.

The interior of the vehicle shall be a safe operating environment for the Crew and all heavy objects shall be carried in secure storage lockers mounted securely to the body.

Seating type is free provided the seats are securely fitted to the body of the vehicle or rollcage.

Any video cameras fitted inside cabins shall be securely fastened in a way not to pose an injury risk to occupants.

With the exception of standard factory fitted heater systems, any engine cooling system line that runs through the cabin must be totally sealed from the occupants by means of a metal or aluminium shield. No, non welded, piping joins shall exist in the cabin area.

4.1.7 SUSPENSION

Except in Open Class powered and manually operated active suspension that adjusts each wheel individually shall not be permitted.

4.1.8 TYRES

Supplementary Regulations shall nominate the allowable tread pattern and tread depth on tyres permitted to be fitted to vehicles in the Event.

Tyres may be fitted with tubes. Valves shall not protrude beyond the outer edge of the tyre profile. Tyre tread shall have a minimum contact ratio of 50%.

4.1.9 WHEELS AND HUBS

Unless specified otherwise in the Supplementary Regulations only vehicles with four wheel drive configuration may be entered in the competitions governed by these Rules.

4.1.10 SPARE WHEELS

Unless specified otherwise in supplementary regulations, a spare wheel shall be carried and securely fastened behind the cargo barrier or on the rear of the vehicle.

4.1.11 WINCHES

Powered winches for all classes except Challenge and Open shall be as purchased from the supplier with limited modifications allowed. Refer to vehicle specifications for details.

Powered winches shall have an operational braking system to prevent unspooling when not in operation and to hold the vehicle when winching pauses.

Winches shall be mounted in a secure manner to the chassis or to an engineered sub-frame in a monocoque vehicle.

Rear Mounted winches shall be permitted provided that the Crew whilst in the cabin are protected and isolated from the cable by 1.5mm minimum thickness of steel or 3mm minimum thickness of aluminium.

Power for winches shall only be taken from the vehicle engine, driveline or vehicle electrical system. Auxiliary engines shall not be permitted.

Hand winches may be used.

Winch cables shall be rated synthetic rope, free of damage or excessive degradation.

Synthetic ropes shall have a load rating of at least twice that of the winch SWL rating or four times the nominal vehicle weight, whichever is the least.

Specifications of the winch cable or rope shall be supplied by the Competitor at scrutineering.

Loops or eyes shall be spliced or professionally swaged and be rated at the same, or greater capacity than, the cable or rope.

Winch hooks must be rated and have a working self-retaining safety catch

Vehicle mounted hooks, eyes and fittings shall have a load rating of at least twice the weight of the vehicle and greater than the winch capacity. Loose coupling hooks or eyes shall be rated at or above the capacity of the cable or rope.

There shall be no sharp protruding pins forward nor upward on the front of the vehicle.

4.1.12 BATTERIES

Batteries are free. Fitting and location of additional batteries is free.

All batteries shall be securely fastened and if they are in the cabin they shall be secured in an enclosure and vented to the outside of the vehicle unless the battery is a sealed unit.

All terminals must be covered by an insulating material.

Production, Challenge and Open Class vehicles shall be fitted with two mechanical battery isolation switches.

The first shall be fitted to winching power system(s) and must terminate all power to the winch(s) when activated.

The second shall be fitted to the vehicle's primary electrical system thus terminating the running of the engine and all primary power throughout the vehicle when activated with the sole exception being a fused, low current circuit that allows activation of an interior light, (max 10w), GPS unit(s) and UHF radio.

Both switches may be incorporated into a single physical isolator.

4.1.13 SUPPLEMENTARY FUEL CONTAINERS

Supplementary fuel containers or auxiliary fuel tanks may be carried or fitted provided they comply with Australian Standard/New Zealand Standard 2906:2001 or international standards of equal or greater specification.

Supplementary fuel containers shall be securely fastened outside of the passenger compartment and away from heat sources such as any type of heat exchanger, exhaust etc.

The original fuel tank may be supplemented, relocated or replaced with a long-range fuel tank.

There shall be a firewall of equivalent material to original between any fuel tank or container and occupants.

4.1.14 FUEL

Fuel used shall be limited to that available from retail fuel outlets.

4.1.15 WINDSCREEN

Should a windscreen be broken during the course of an event, the vehicle may continue, at the discretion of the Event Scrutineer or Chief Scrutineer, in consultation with the Event Organiser, under the following conditions;

(a) all loose glass has been removed from the vehicle; and

(b) the crew shall compete, thereafter, wearing long sleeves long pants and goggles, or a helmet with a full face visor.

Note: The Event Scrutineer or Chief Scrutineers decision takes precedence.

4.2 STANDARD CLASS

4.2.1 TYRES

Tyre sizes shall be limited to those originally specified by the vehicle manufacturer.

Tyres shall be enclosed within the width of mudguards and flares as viewed from directly above.

4.2.2 WHEELS

Wheels shall be limited to the same size and offset as those originally specified by the vehicle manufacturer for the vehicle.

Bead locks shall not be permitted.

Wheel spacers shall not be permitted.

4.2.3 AXLES

Axle housings shall be original but may be protected.

Axle internal components shall be original and extension breathers may be fitted.

4.2.4 BRAKES

The original braking system shall be retained unmodified except for the composition of the brake pads/linings.

The handbrake (park brake) shall be as originally fitted and shall be operational.

4.2.5 DIFFERENTIALS

The original differential system shall be retained unmodified.

Differential ratios shall not be changed.

Should differential locks be fitted to the vehicle as original equipment the vehicle shall not be eligible to compete in this class but may be eligible to compete in any class above this vehicle class.

4.2.6 GEARBOX

The original gearbox shall be retained unmodified.

4.2.7 TRANSFER CASE

The original transfer case system shall be retained unmodified.

Transfer case ratios shall be those available for that vehicle model and year.

The transfer case and differentials shall be operative and capable of driving all four wheels.

4.2.8 STEERING

Original steering boxes shall be retained in their original location and shall be unmodified.

Original steering linkages and components shall be retained unmodified but may have protection fitted to them.

4.2.9 SUSPENSION

Shock absorbers and bushes shall be retained unmodified (except for normal service replacement with non-genuine parts) and the original number and location shall be retained.

The original spring design shall be retained unmodified. Original suspension linkages/pins shall be retained unmodified.

4.2.10 ENGINE

The original engine shall be retained unmodified.

Snorkels and aftermarket air filters are free.

Oil filters are free except the original method of attachment shall be retained.

4.2.11 ELECTRICAL

Alternators shall be retained unmodified.

Headlight inserts and globes are free.

Tail lights, stop lights and reversing lights shall be retained unmodified.

4.2.12 EXHAUST SYSTEMS

The original exhaust system shall be retained unmodified except for normal service replacement with non-genuine parts.

4.2.13 CHASSIS

The original chassis shall be retained unmodified.

Protection plates may be added for protection but not for strengthening purposes.

4.2.14 BODY

The original body shall be retained unmodified except that additional mud flaps, door sills and flares may be added for protection.

4.2.15 LIGHTS

Vehicles may be fitted with up to four driving lights in addition to the original headlights and driving lights.

Driving lights shall be mounted at the front of the vehicle and shall operate with the high/low beam switch in conjunction with the headlights and shall be fitted in accordance with relevant registration requirements.

A brake and tail light, or lights, shall be installed and working.

In events where there are multiple vehicles on course at night, all vehicles are to be fitted with an additional, flashing red or amber light, with its own power supply, mounted on the rear of the vehicle as close as practical to the roof line. Clearly visible from 100m away at night with a flash rate of 1 flash per second minimum. (e.g. bike light etc). This light shall be activated prior to starting the stage to allow approaching vehicles to identify that a vehicle is ahead even with the electrical system isolated.

It is strongly recommended that a dust lamp be fitted to the rear of the vehicle.

4.2.16 FUEL

The original fuel system shall be retained.

4.2.17 WINCHES

All vehicles must be fitted with one (and only one) operational powered winch when competing in an event where winching is required.

Electric winches may have some additional bracing, improved bearings, improved lubrication and an alternative DC motor with upgraded controls, but must remain in its original configuration. PTO and Hydraulic winches must be as designed by the supplier.

4.3 TOURING CLASS

4.3.1 TYRES

Tyre sizes shall be limited to 33x12.5 or metric equivalent and must pass through a gauge with internal dimensions of 840mm and 320mm at 20psi or 1.4bar.

Track increase shall be limited to 50mm.

Tyres shall be enclosed within the width of mudguards and flares as viewed from directly above.

4.3.2 WHEELS

Wheels shall be limited to a rim width of 203mm (8") and any offset shall be such that the tyre shall be enclosed within the width of mudguards and flares.

Bead locks shall not be permitted.

Wheel spacers shall not be permitted.

4.3.3 AXLES

Axle housings shall be original but may be protected.

Axle internal components shall be original and extension breathers may be fitted.

4.3.4 BRAKES

The original braking system shall be retained unmodified except for the composition of the brake pads/linings.

The handbrake (park brake) shall be as originally fitted and shall be operational.

4.3.5 DIFFERENTIALS

The original differential system shall be retained unmodified except that differential locks may be fitted.

Differential ratios are free.

4.3.6 GEARBOX

The original gearbox shall be retained unmodified.

4.3.7 TRANSFER CASE

The original transfer case system shall be retained unmodified.

Transfer case ratios shall be those available for that vehicle make.

The transfer case and differentials shall be operative and capable of driving all four wheels.

4.3.8 STEERING

Original steering boxes shall be retained in the original location and shall be unmodified.

Original steering linkages and components shall be retained unmodified but may have protection fitted to them.

4.3.9 SUSPENSION

Selection of shock absorbers and bushes is free except that the original number and location shall be retained.

The original spring design shall be retained except that components may be modified or replaced with aftermarket units.

Original suspension linkage or pin configuration, length and positioning shall be retained but may be strengthened.

Vehicle may be lifted by up 50mm from standard trim height.

4.3.10 ENGINE

The original engine shall be retained unmodified except that alternative engine management systems, and in the case of diesel engines an aftermarket turbocharger, supercharger and intercooler, may be fitted.

Snorkels and aftermarket air filters are free.

Oil filters are free except the original method of attachment shall be retained.

4.3.11 ELECTRICAL

Original alternators shall be retained unmodified.

Headlight inserts and globes are free.

Tail lights, stop lights and reversing lights shall be as originally fitted.

4.3.12 EXHAUST SYSTEMS

The exhaust system is free.

Exhaust systems on tray back utes must terminate a minimum of 300mm past the rear of the cab. Exhaust systems on all other vehicles must terminate past the rear diff.

4.3.13 CHASSIS

The original chassis shall be retained unmodified.

Strengthening with the additional bracing is permitted.

Spring and shock absorber mountings may be strengthened.

Protection plates may be added for strengthening and protection purposes.

4.3.14 BODY

The original body shall be retained unmodified except that additional mud flaps, door sills and flares may be added for protection.

4.3.15 LIGHTS

Vehicles may be fitted with up to four driving lights in addition to the original headlights and driving lights.

Driving lights shall be mounted at the front of the vehicle and shall operate with the high/low beam switch in conjunction with the headlights and shall be fitted in accordance with relevant registration requirements.

A brake and tail light, or lights, shall be installed and working.

In events where there are multiple vehicles on course at night, all vehicles are to be fitted with an additional, flashing red or amber light, with its own power supply, mounted on the rear of the vehicle as close as practical to the roof line. Clearly visible from 100m away at night with a flash rate of 1 flash per second minimum. (e.g. bike light etc). This light shall be activated prior to starting the stage to allow approaching vehicles to identify that a vehicle is ahead even with the electrical system isolated.

It is strongly recommended that a dust lamp be fitted to the rear of the vehicle.

4.3.16 FUEL

The original fuel system shall be retained.

Alternative fuel such as LPG may be fitted in accordance with State and National Regulations.

4.3.17 WINCHES

All vehicles must be fitted with one (and only one) operational powered winch when competing in an event where winching is required.

Electric winches may have some additional bracing, improved bearings, improved lubrication and an alternative DC motor with upgraded controls, but must remain in its original configuration.

PTO and Hydraulic winches they must be as designed by the supplier.

4.4 PRODUCTION CLASS

4.4.1 TYRES

Tyre sizes shall be limited to 35.5 x 13.5 and must pass through a gauge with internal dimensions of 900mm and 343mm at 20psi or 1.4bar.

Tyres shall be enclosed within the width of mudguards and flares

4.4.2 WHEELS

Wheel options are unlimited, except that tyres shall be enclosed within the width of mudguards and flares as viewed from directly above.

Bead locks are allowed.

Wheel spacers shall not be permitted.

4.4.3 AXLES, HOUSINGS AND HUBS

Axle housings shall be the original equipment for that series/model of vehicle except that they may be strengthened and protected.

Extension breathers may be fitted.

Hubs are free except that drop axles and reduction hubs shall not be permitted.

4.4.4 BRAKES

The braking system may be modified provided that the braking performance of the vehicle meets or exceeds the vehicle's original braking performance.

The composition of the brake pads/linings is free.

A secondary or emergency braking system (a.k.a. park brake) shall be fitted and shall be operational.

4.4.5 DIFFERENTIALS

The differential system for that series/model of vehicle shall be retained unmodified except that differential locks may be fitted.

Differentials may be strengthened which includes up rated axles and CV's.

Differential ratios are free.

4.4.6 GEARBOX

Gearboxes shall be the original equipment for that make of vehicle except that they may be strengthened and protected and they may be replaced by an automatic or manual from that make of vehicle.

Gearbox ratios are free.

4.4.7 TRANSFER CASE

The transfer case system for that make of vehicle shall be retained except that transfer case ratios are free.

4.4.8 STEERING

Steering boxes shall be limited to power assisted mechanical system. No External power assisted ram to be used.

Steering linkages and components are free and may have protection fitted to them.

4.4.9 SUSPENSION

One shock absorber and spring per corner (4 in total)

One spring per corner and independent of shock absorber

Shock absorber and spring be mounted in the original mount. Shock absorber tower may be spaced to allow for the fitting of a taller shock absorber.

Suspension lift shall be limited to a maximum of 100mm from original.

Control arm configuration shall be retained including mounting points but may be strengthened. Arm length may be extended by a maximum of 20mm.

Panhard Rod configuration shall be retained including mounting points but may be strengthened. Arm length may be extended by a maximum of 20mm and may be adjustable.

The axle position shall be retained within 50 mm of its original location to retain substantially the same wheel base.

4.4.10 ENGINE

The original engine for that vehicle series/model shall be retained unmodified except that alternative engine management systems can be used.

Diesel engines may be fitted with a factory turbocharger, factory fitted intercoolers may be retained. Alternatively, diesel engines may be fitted with an aftermarket turbocharger on a cast iron manifold without an intercooler.

Petrol engines, no intercoolers, superchargers or turbochargers to be fitted.

Snorkels and aftermarket air filter arrangement are free.

Oil filters are free except the original method of attachment shall be retained.

4.4.11 ELECTRICAL

Alternators are free.

Headlight inserts and globes are free.

Taillights, stop lights and reversing lights are free.

4.4.12 EXHAUST SYSTEMS

The exhaust system is free.

Exhaust systems on tray back Ute must terminate a minimum of 300mm past the rear of the cab below door handle height on a downward angle at the extremities of the vehicle.

Exhaust systems on all other vehicles must terminate past the rear diff below door handle height on a downward angle at the extremities of the vehicle.

4.4.13 CHASSIS

The original chassis for that model of vehicle shall be retained unmodified.

Strengthening with the additional bracing is acceptable.

Spring and shock absorber mountings may be strengthened.

Protection plates may be added for strengthening and protection purposes.

4.4.14 BODY

The body of the make and model of the vehicle shall be retained but can be modified.

All glass must be retained in the body forward of the B pillar with door trims fitted to front doors.. Polycarbonate or Acrylic (Perspex) may be used rear of the B pillar provided that it is approved for automotive use and must be see through.

Body lifts shall be limited to 50mm and shall be completed in a safe and secure manner and the spacers shall be a non-crushable material and the bolts shall be the same diameter as the original body mounting bolts and at least grade 8.8 There shall be the same number of body mounts and supports as the original chassis and body combination.

Aftermarket composite or fibreglass panels are not permitted. Original engine bay internal structure shall be retained.

Wheel base must remain standard

Additional mudflaps, flares and door sills may be added for protection.

4.4.15 LIGHTS

Vehicles may be fitted with any number of supplementary lights including driving lights and light bars etc.

All additional lighting must dip by means of a single action, be it the original high beam 'stalk' or an additional single switch other than the vehicle or winch isolator.

Low beam headlights of some sort, providing suitable light to operate the vehicle without impeding others, must be retained.

Long course events where the vehicle is to be driven on a public road require the vehicle to have functioning low beam headlights with a standard low beam pattern. A brake and tail light, or lights, shall be installed and working.

In events where there are multiple vehicles on course at night, all vehicles are to be fitted with an additional, flashing red or amber light, with its own power supply, mounted on the rear of the vehicle as close as practical to the roof line. Clearly visible from 100m away at night with a flash rate of 1 flash per second minimum. (e.g. bike light etc). This light shall be activated prior to starting the stage to allow approaching vehicles to identify that a vehicle is ahead even with the electrical system isolated.

It is strongly recommended that a dust lamp be fitted to the rear of the vehicle.

4.4.16 FUEL

The original fuel system shall be retained.

Fuel tanks shall have devices installed so as to not leak in case of roll over, but still allow the tank to breathe as needed. A physical rollover valve or routing of fuel tank breather to rise above the tank a distance equal to (or greater than) the deepest dimension of the tank, across the tank to the side opposite of attachment and be downwards to a point at least 150mm below the lowest point of the tank.

Alternative fuel such as LPG may be fitted in accordance with State and National Regulations. LPG may be fitted to petrol engines only.

4.4.17 WINCHES

All vehicles must be fitted with one (and only one) electrically operated 12volt winch when competing in an event where winching is required. Winch must be front mounted

Winches may have some additional bracing, improved bearings, improved lubrication, upgraded brake shaft & upgraded controls, but must remain in its original configuration with standard gear ratios. Upgraded wiring is permitted.

Motor must be a standard commercially available unmodified winch motor.

Winch must be fitted with one (1) 12 volt winch motor and winch can run on 12 volt system only.

Winch must retain standard drum in length & diameter, however you may upgrade the flange thickness

No remotely operated free spools

Synthetic type winch rope must be used.

4.5 WEEKEND WARRIOR CLASS

PREFACE/CLASS COMMENTARY.

Weekend Warrior (WW) class is intended to allow new and prospective competitors to get into Winch Challenge in a safe and controlled environment. As such, the course design and speeds shall be regulated to mitigate risk in this developmental racing environment.

Vehicle specifications have been made to be as open as possible so as to allow people to run their modified “bush” vehicles on the understanding that this is intended as a stepping stone into the full specification Modified production and Challenge classes.

To maintain this class being an entry level into full specification Modified production or Challenge Class, the old VicWinch rule is to be applied with regards to winners. If you win a WW class race from 2023 season onwards as a driver, you are then ineligible to race that class after the completion of that season. So simply, you win round 1, you keep racing for that year, but step up the next.

COURSE DESIGN:

Being an entry level class, all courses shall be risk mitigated to exclude high risk settings involving large drops, high speed and likely vehicle damage. The courses will be focused on technique in a controlled and safe environment. All courses will be designed and/or approved by the Event Organizer and a suitably experienced CCDA Representative prior to ANY Weekend Warrior cars entering the course. Documentation of this having taken place is to be maintained and provided to CCDA.

The first stage for any Weekend Warrior event (to be completed by all Weekend Warrior entrants before heading to another stage) will consist of a “drill” that will be timed and include a minimum of:

- Start and finish box,
- A number of progress gates/bunting,
- A flat or low intensity winching section,
- A “stop and go” box, and
- Course directional signage.

This first stage will be designed to get the teams into the swing of things and to allow teams to settle into the event in the easiest way possible. It provides opportunity for safe winching practices, safe vehicle exit and entry, basic driving ability assessment and an understanding of course signage. If any course marshals feel that competitors may have struggled with this first stage, they must contact the Event Organiser and CCDA representative and advise their concerns. The Event Organiser and CCDA representative shall discuss these concerns with the competitor to remedy the issues, which may include the competitor being disallowed to continue the event.

DRIVERS:

Drivers and Co-drivers/Navigators will all hold full CCDA membership and be suitably licensed in their home state or territory to drive a registered roadgoing motor vehicle.

Drivers will also be required to have undertaken CCDA driver assessment or demonstrated prior experience.

Drivers and co-drivers are required to disclose any medical conditions that may impede with their ability to both operate during competition and withstand the possible impacts associated with our motorsport. Both driver and Co-Driver are subject to the same “Closed door vehicle exit” test as other classes.

STOP THE CLOCK:

On the basis of this class allowing newcomers to get a feel for the sport, a “stop the clock” can be issued by either course officials or a team member without penalty. This “stop the clock” may not be used for any competitive advantage, however it allows teams time to re-think a situation in the effort of safety while not under the pressure of time.

SPORTING REGULATIONS / COMPETITION RULES:

Refer to Current rules

VEHICLE SCRUTINEERING:

All Weekend Warrior vehicles will be subject to Event Scrutineering prior to commencing competition. This is the same check as other classes. (Refer to CCDA website for Weekend Warrior scrutineering document)

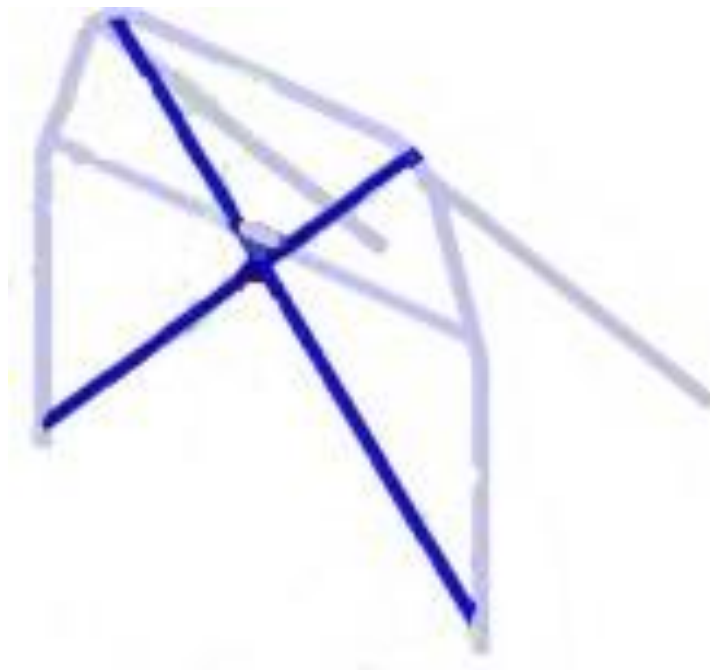
4.5.1 OCCUPANT PROTECTION:

All vehicles shall be fitted with at least a 4 point roll over protection system (ROPS) consisting of:

- All tube material is 44.5mm 2.5mm wall CDA as per CCDA ROPS Rule 5.9

The Rollover Protection for Weekend Warrior shall consist of:

- Main hoop with cross brace, secured at the base with specifications matching CCDA ROPS Rule 5.5



Sample Four Point ROPS

An additional tab is to connect the Main Hoop to the vehicle body shell by picking up the upper seat bolt mounting point. This tab is to be fully welded to the main hoop and at least 6mm thick. Allowance for this material must be considered when re-attaching the seat belt.

Rear stays attached via full weld or removable FIA approved Camburg style joiner or 4 bolted slip-joint. e.g.



Note: Pics courtesy of www.rollcagejoiners.com.au

Rear stays are to be attached to body/chassis via methods approved in CCDA ROPS Rule 5.5

In the case where a ute is fitted with a tray, considerations may be made by the CCDA Chief Scrutineer and arranged by the competitor prior to the event.

4.5.2 DOORS

All vehicles will be fitted with factory style doors, they must be fully operable to start any stage. They must be latched firmly closed on the door latches for any occupants to be in the vehicle while moving or to finish a stage.

4.5.3 SEATS AND SEAT BELTS

Seat belts must be in working order, locking mechanism's operational, with no damage to the belts or mechanisms. Harnesses, if installed to the vehicle may be used.

Seats must be in good condition and not loose with headrests fitted.

4.5.4 FUEL

Commercially available (from a petrol station) pump fuels only. No race fuels, no methanol injection etc allowed

4.5.5 IGNITION/KILL SWITCH

Vehicle engine may be shut down via either or both, Ignition key or Mechanical Kill switch. It must shut the engine down in the case of emergency at 2,500RPM

All vehicle specifications unless specified above are to be in accordance with CCDA Challenge Class rules.

4.6 MODIFIED PRODUCTION CLASS

PREFACE/SPIRIT OF THE CLASS.

The intention of this class is to provide competitors with a grass roots, entry level option for Winch Challenge, with an aim to keep the cost of racing down to a minimum without sacrificing vehicle and occupant safety. This class is largely designed to reduce the advantage of anyone having a big budget. If an alteration is not listed in the rules below, then it is considered not allowed.

Rule amendments must be passed by the CCDA and can only come into effect in the next calendar year of racing.

DEFINITIONS:

4WD vehicle: A vehicle that was made available in the Australian market, compiled by the manufacturer or representative with a high/low transfer case from factory.

Make: Original manufacturer of the vehicle.

Series: The series of vehicle is defined as the marketing series

- Patrol MQ, GQ, GU, Y62 etc.
- Landcruiser 40, 60, 80, 100 etc.

Model: Defined as the model designation

- Patrol, GQ is different to a GU
- Landcruiser 80 is different to a 100

Note: Rollover Protection for Modified Production class is a 6 point cage as per Rule 4.1.3 above.

4.6.1 TYRES

Tyre sizes shall be limited to 37.5 x 13.5 and must pass through a gauge with internal dimensions of 950mm and 350mm at 20psi or 1.4bar.

A minimum of 70% of the tyres shall be enclosed within the width of mudguards, flares and/or barwork viewed from directly above at scrutineering.

4.6.2 WHEELS

Wheel options are unlimited, except that tyres shall be 70% enclosed within the width of mudguards and flares as viewed from directly above.

Bead locks are allowed.

Wheel spacers shall not be permitted.

4.6.3 AXLES, HOUSINGS AND HUBS

The rear axle housing shall be the original equipment for that series/model of vehicle except that they may be strengthened and protected.

The front axle housing may be changed using an alternate housing from another make, model or series. This housing must be an original equipment housing off a full production vehicle available to the Australian market. Aftermarket housings are not permitted.

Strengthened CV's and axles may be fitted

Extension breathers may be fitted.

Hubs are free except that drop axles and reduction hubs shall not be permitted.

4.6.4 BRAKES

The braking system may be modified provided that the braking performance of the vehicle meets or exceeds the vehicle's original braking performance.

The composition of the brake pads/linings is free.

The factory secondary or emergency braking system (a.k.a. park brake) shall be fitted and shall be operational.

Cutting brakes are not allowed

4.6.5 DIFFERENTIALS

The rear differential system for that make, model and series of vehicle shall be retained unmodified except that differential locks may be fitted.

Front differential may be from another make model or series as per Rule 4.4.3 above however the differential shall be retained unmodified except that differential locks may be fitted.

Differentials may be strengthened which includes up rated axles and CV's.

Differential ratios are free.

4.6.6 GEARBOX

Gearboxes shall be the original equipment for that make, model and series of vehicle except that they may be strengthened and protected and they may be replaced by an automatic or manual from that make and model of vehicle. Automatic transmissions may be manualized.

Gearbox ratios are free.

4.6.7 TRANSFER CASE

The transfer case system for that make, model and series of vehicle shall be retained.

Transfer case ratios are free.

4.6.8 STEERING

Steering boxes shall be limited to power assisted mechanical system. No external power assisted ram to be used. The box may be swapped with an alternate unit, however it must be bolted to the factory mounting location. Changing the bolt pattern for the box in the chassis is allowed, however the same location must be used

Steering linkages and components are free and may have protection fitted to them.

4.6.9 SUSPENSION

Springs must only be mounted to factory chassis mounts. The chassis mounts must be in original location and may only be strengthened. They may not be lifted or have their positions changed.

Only one shock absorber per corner mounted to factory chassis mounts. Mounts may be strengthened but must be in factory location. Factory shock towers that allow a bolt in spacer to be fitted can be installed to allow a longer shock. These spacers must be able to bolt to existing mountings and shall be no longer than 50mm.

Remote reservoir shocks may be used. Bypass shocks may not be used. Coil-over shocks may not be installed unless the vehicle came with factory coil-over shocks.

The use of adaptors to change the mounting design of shocks is not permitted.

Hydraulic bump stops are not allowed.

The addition of steel fabricated plate on the chassis and diff housing to prevent the axle from hitting the engine sump in the case of full suspension failure is allowed.

Control arm configuration and location shall be retained including mounting points but may be strengthened. Bolt on drop boxes may be fitted provided that the wheelbase stays within

specification. "Hybrid" style front lower control arms are allowed in conjunction with drop boxes provided that the wheelbase stays within specification for the make and model.

Arm length may be extended by a maximum of 20mm.

Panhard Rod configuration shall be retained including mounting points but may be strengthened. Arm length may be extended by a maximum of 20mm and may be adjustable.

The axle position shall be retained within 50 mm of its original location to retain substantially the same wheel base.

4.6.10 ENGINE

The original make engine for that vehicle can be used, so long as it is within 0.3L capacity, the same cylinder arrangement and number as what was fitted to the core make, model and series of vehicle and using the same induction type and sourced from a factory 4WD vehicle that was available in the Australian market. Thus allowing a GQ patrol that came with a TB42 to be fitted with a TB45, A Suzuki that came with a G13A to be fitted with a G16B, but not allowing a GQ that came with a TB42 inline 6 to be fitted with a VK56 V8.

Alternative engine management systems can be used, camshafts, pistons, rods etc. installed, however the factory block capacity must remain standard and factory spec crank shaft must be retained.

Diesel engines may be fitted with a factory turbocharger, factory fitted intercoolers may be retained. No aftermarket intercooler, supercharger or turbocharger to be fitted.

Maximum alternator charge rate allowed of 28.8V

Snorkels and aftermarket air filter arrangement are free.

Oil filters are free except the original method of attachment shall be retained.

4.6.11 ELECTRICAL

Alternators are free. Maximum alternator charge rate allowed of 28.8V

Headlight inserts and globes are free.

Taillights, stop lights and reversing lights are free.

4.6.12 EXHAUST SYSTEMS

The exhaust system is free.

Exhaust systems on tray back Ute must terminate a minimum of 300mm past the rear of the cab below door handle height on a downward angle at the extremities of the vehicle.

Exhaust systems on all other vehicles must terminate past the rear diff below door handle height on a downward angle at the extremities of the vehicle.

4.6.13 CHASSIS

The chassis of the make, model and series of vehicle shall be retained but can be modified.

The chassis must remain factory standard apart from strengthening from the rear axle forward. It may not be trimmed, bent or modified. The front of the chassis may not be removed or altered apart from strengthening. Crush cones may be removed from the tip of chassis.

Spring and shock absorber mountings may be strengthened.

Protection plates may be added for strengthening and protection purposes.

Wheel base must remain standard as per Rule 4.5.9.

4.6.14 BODY

The body of the make, model and series of vehicle shall be retained but can be modified.

All panels must be retained in the body forward of the B pillar.

The front of the vehicle panels may be "bobbed" however a full factory width grille and factory style headlights must be retained. The grille may be modified to accommodate winch fitting.

Body lifts shall be limited to 50mm and shall be completed in a safe and secure manner. Body spacers shall be a non-crushable material and the bolts shall be the same diameter as the original body mounting bolts and at least grade 8.8 There shall be the same number of body mounts and supports as the original chassis and body combination.

Aftermarket composite or fibreglass panels are not permitted forward of the B pillar.

Original engine bay internal structure shall be retained. There may be no modification for body length between the B pillar and the radiator support panel.

The body may be trimmed for weight reduction or tyre clearance, however the A and B pillars and floor must remain standard from the B pillar forward. Door sills may be replaced and strengthened.

Front and rear guards can be trimmed for tyre clearance, the rear body behind the B pillar may be modified or removed provided that a rear wall meeting specification and wheel guards are installed.

Additional mudflaps, flares and door sills may be added for protection.

Radiator may be rear mounted, coolant hoses must be outside of the passenger compartment to protect occupants

4.6.15 LIGHTS

Vehicles may be fitted with any number of supplementary lights including driving lights and light bars etc.

All additional lighting must dip by means of a single action, be it the original high beam 'stalk' or an additional single switch other than the vehicle or winch isolator.

Low beam headlights of some sort, providing suitable light to operate the vehicle without impeding others, must be retained. A brake and tail light, shall be installed and working.

In events where there are multiple vehicles on course at night, all vehicles are to be fitted with an additional, flashing red or amber light, with its own power supply, mounted on the rear of the vehicle as close as practical to the roof line. Clearly visible from 100m away at night with a flash rate of one(1) flash per second minimum. (e.g. bike light etc). This light shall be activated prior to starting the stage to allow approaching vehicles to identify that a vehicle is ahead even with the electrical system isolated.

It is strongly recommended that a dust lamp be fitted to the rear of the vehicle.

4.6.16 FUEL

The original fuel system shall be retained.

Fuel tanks shall have devices installed so as to not leak in case of roll over, but still allow the tank to breathe as needed. A physical rollover valve or routing of fuel tank breather to rise above the tank a distance equal to (or greater than) the deepest dimension of the tank, across the tank to the side opposite of attachment and be downwards to a point at least 150mm below the lowest point of the tank.

Alternative fuel such as LPG may be fitted in accordance with State and National Regulations. LPG may be fitted to petrol engines only.

Only pump fuel may be used available at any fuel station. No racing fuels. No methanol or similar injection.

4.6.17 WINCHES

All vehicles must be fitted with one (and only one) electrically operated 12v or 24V commercially available winch when competing in an event where winching is required. Winches may have some additional bracing, improved bearings, improved lubrication, upgraded brake shaft and upgraded controls, altered ratios, air free spool and upgraded wiring is permitted. Housings may be replaced with a commercially available unit.

Single speed winch transmissions only.

The winch braking system must be one that is commercially available.

Motor must be a standard commercially available unmodified winch motor. Brushed winch motors only.

One (1) winch motor only limited to a maximum commercial rating of 9hp.

Winch rope length is limited to 50m

Winch must be front mounted

Synthetic type winch rope shall be used.

A maximum of two batteries to run the winch size limited to 350mm case length.

Batteries are restricted to Lead, AGM style or Calcium batteries. Lithium Polymer batteries etc are not permitted.

4.7 CHALLENGE CLASS

4.7.1 TYRES

Tyre sizes shall be limited to 37.5 x 13.5 and must pass through a gauge with internal dimensions of 950mm and 350mm at 20psi or 1.4bar.

A minimum of 70% of the tyres shall be enclosed within the width of mudguards and flares as viewed from directly above when the wheels are pointing straight ahead.

4.7.2 WHEELS

Wheel options are unlimited.

Bead locks are allowed.

Wheel spacers are permitted to a maximum width of 40mm.

4.7.3 AXLES AND HUBS

Axle housings are free.

Axle internal components are free.

Hubs are free.

4.7.4 BRAKES

The braking system may be modified provided that the braking performance of the vehicle meets or exceeds the vehicle's original braking performance.

The composition of the brake pads/linings is free.

A secondary or emergency braking system (a.k.a park brake) shall be fitted and shall be operational.

Additional independent braking systems (Cutting brakes) are permitted, providing they do not impede the operation of the primary braking system.

4.7.5 DIFFERENTIALS

The differential system is free.

Differentials may be modified and strengthened.

Differential ratios are free.

4.7.6 GEARBOX

The gearbox options are open.

4.7.7 TRANSFER CASE

The transfer case system is free.

Transfer case ratios are free.

The transfer case and differentials shall be operative and capable of driving all four wheels.

4.7.8 STEERING

Steering is free. Where steering boxes are used they shall be securely fastened to the chassis.

Steering linkages and components are free and may have protection fitted to them.

4.7.9 SUSPENSION

Selection of shock absorbers and bushes is free including number and location.

The spring configuration and position on the chassis is free.

Control arm configuration is free including arm length and mounting points.

Suspension mounting points may be strengthened.

The axle position is free.

4.7.10 ENGINE

Any replacement engine is permissible.

Engine management systems are free.

Snorkels and aftermarket air filters are free.

Oil filters are free.

4.7.11 ELECTRICAL

Alternators are free.

Fitment of additional electrical systems (such as independent 24v systems) is permitted. Provided they are less than 36 volt.

Headlight inserts and globes are free.

Tail lights, stop lights and reversing lights are free.

4.7.12 EXHAUST SYSTEMS

The exhaust system is free.

Exhaust systems on tray back ute must terminate a minimum of 300mm past the rear of the cab below door handle height on a downward angle.

Exhaust systems on all other vehicles must terminate past the rear diff below door handle height on a downward angle at the extremities of the vehicle.

4.7.13 CHASSIS

The original chassis for that make of vehicle shall be used. The chassis may be modified however, the original chassis must remain intact from the front firewall to the rear passenger compartment bulkhead.

Strengthening with the additional bracing is acceptable.

Spring and shock absorber mountings may be strengthened.

Protection plates may be added for strengthening and protection purposes.

4.7.14 BODY

The body of the make of the vehicle shall be readily recognised and shall be the same manufacturer as the chassis.

The combination of body and chassis may be updated with body and chassis elements from later models of the same make of vehicle.

Body lifts shall be limited to 75mm and shall be completed in a safe and secure manner and the spacers shall be a non-crushable material and the bolts shall be the same diameter as the original body mounting bolts and at least grade 8. There shall be the same number of body mounts and supports as the original chassis and body combination.

Additional mudflaps flares and door sills may be added for protection.

4.7.15 LIGHTS

Vehicles may be fitted with any number of supplementary lights including driving lights and light bars etc.

All additional lighting must dip by means of a single action, be it the original high beam 'stalk' or an additional single switch other than the vehicle or winch isolator.

Low beam headlights of some sort, providing suitable light to operate the vehicle without impeding others, must be retained.

Long course events where the vehicle is to be driven on a public road require the vehicle to have functioning low beam headlights with a standard low beam pattern.

A brake and tail light, or lights, shall be installed and working.

In events where there are multiple vehicles on course at night, all vehicles are to be fitted with an additional, flashing red or amber light, with its own power supply, mounted on the rear of the vehicle as close as practical to the roof line. Clearly visible from 100m away at night with a flash rate of 1 flash per second minimum. (e.g. bike light etc). This light shall be activated prior to starting the stage to allow approaching vehicles to identify that a vehicle is ahead even with the electrical system isolated.

It is strongly recommended that a dust lamp be fitted to the rear of the vehicle.

4.7.16 FUEL

The fuel system is free but shall be in a sound condition with all fittings tightly fastened.

Fuel tanks shall have devices installed so as to not leak in case of roll over, but still allow the tank to breathe as needed. A physical rollover valve or routing of fuel tank breather to rise above the tank a distance equal to (or greater than) the deepest dimension of the tank, across the tank to the side opposite of attachment and be downwards to a point at least 150mm below the lowest point of the tank.

Alternative fuel such as LPG may be fitted in accordance with State and National Regulations.

4.7.17 WINCHES

All vehicles must be fitted with operational powered winch(s) when competing in an event where winching is required.

Electric winches may be modified including multiple motors and gearboxes and upgraded controls. PTO and Hydraulic winches may be modified.

Synthetic type winch rope must be used.

4.8 OPEN CLASS

4.8.1 TYRES

Tyre size is free.

4.8.2 WHEELS

Wheel options are unlimited.

Bead locks are allowed.

4.8.3 AXLES AND HUBS

Axle housings are free.

Axle internal components are free.

Hubs and hub spacers are free.

4.8.4 BRAKES

The braking system may be modified provided that the braking performance of the vehicle meets or exceeds the vehicle's original braking performance.

The composition of the brake pads/linings is free.

A secondary or emergency braking system (a.k.a. park brake) shall be fitted and shall be operational.

4.8.5 DIFFERENTIALS

The differential system is free.

Differentials may be modified and strengthened.

Differential ratios are free.

4.8.6 GEARBOX

The gearbox options are free.

4.8.7 TRANSFER CASE

The transfer case system is free.

Transfer case ratios are free.

The transfer case and differentials shall be operative and capable of driving all four wheels.

4.8.8 STEERING

Steering boxes are free but shall be securely fastened to the chassis.

Steering linkages and components are free and may have protection fitted to them.

4.8.9 SUSPENSION

Selection of shock absorbers and bushes is free including number and location.

The spring configuration and position on the chassis is free.

Control arm configuration is free including arm length and mounting points.

Suspension mounting points may be strengthened.

The axle position is free.

Independently adjustable suspension is free

4.8.10 ENGINE

Any replacement engine is permissible.

Engine management systems are free.

Snorkels and aftermarket air filters are free.

Oil filters are free.

4.8.11 ELECTRICAL

Alternators are free.

Headlight inserts and globes are free.

Taillights stoplights and reversing lights are free.

4.8.12 EXHAUST SYSTEMS

The exhaust system is free.

Exhaust systems on tray back Ute must terminate a minimum of 300mm past the rear of the cab below door handle height on a downward angle at the extremities of the vehicle.

Exhaust systems on all other vehicles must terminate past the rear diff below door handle height on a downward angle at the extremities of the vehicle.

4.8.13 CHASSIS

The chassis is free.

Chassis construction shall meet or exceed the current CCDA Challenge Class Rollover Protection System (ROPS) strength requirements. Suitable engineering certification shall be provided for scrutineers to confirm this requirement.

4.8.14 BODY

The body is free.

Body lifts are free and shall be completed in a safe and secure manner and the spacers shall be a non-crushable material and the bolts shall be at least the same diameter as the original body mounting bolts and at least grade 8.

Additional mudflaps flares and doorsills may be added for protection.

4.8.15 LIGHTS

Vehicles may be fitted with any number of supplementary lights including driving lights and light bars etc.

All additional lighting must dip by means of a single action, be it the original high beam 'stalk' or an additional single switch other than the vehicle or winch isolator.

Low beam headlights of some sort, providing suitable light to operate the vehicle without impeding others, must be retained.

Long course events where the vehicle is to be driven on a public road require the vehicle to have functioning low beam headlights with a standard low beam pattern. A brake and tail light, or lights, shall be installed and working.

In events where there are multiple vehicles on course at night, all vehicles are to be fitted with an additional, flashing red or amber light, with its own power supply, mounted on the rear of the vehicle as close as practical to the roof line. Clearly visible from 100m away at night with a flash rate of 1 flash per second minimum. (e.g. bike light etc). This light shall be activated prior to starting the stage to allow approaching vehicles to identify that a vehicle is ahead even with the electrical system isolated.

It is strongly recommended that a dust lamp be fitted to the rear of the vehicle.

4.8.16 FUEL

The fuel system is free but shall be in a sound condition with all fittings tightly fastened.

Fuel tanks shall have devices installed so as to not leak in case of roll over, but still allow the tank to breathe as needed. A physical rollover valve or routing of fuel tank breather to rise above the

tank a distance equal to (or greater than) the deepest dimension of the tank, across the tank to the side opposite of attachment and be downwards to a point at least 150mm below the lowest point of the tank. Alternative fuel such as LPG may be fitted in accordance with State and National Regulations.

4.8.17 WINCHES

All vehicles must be fitted with operational powered winch(s) when competing in an event where winching is required.

Electric winches may be modified including multiple motors and gearboxes and upgraded controls. PTO and Hydraulic winches may be modified.

Synthetic type winch rope must be used.

MANUAL OF 4WD MOTOR SPORT
SECTION 5 – ROLLOVER PROTECTION



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5 ROLLOVER PROTECTION

5.1 APPLICATION

Rollover protection is required as specified in the CCDA Class Specifications and Event Supplementary Rules.

5.2 DEFINITIONS

5.2.1 ROPS (ROLL OVER PROTECTION SYSTEM)

A structural framework designed to prevent serious body shell deformation in the event of a collision or the vehicle turning over.

5.2.2 MAIN ROLL BAR (MAIN HOOP)

Structure consisting of a near vertical frame or hoop located across the vehicle just behind the front seats.

5.2.3 FRONT ROLL BAR

A longitudinal bar - its shape following the roofline and A pillars connecting the main hoop to the floor at the base of the A pillar.

5.2.4 LOWER FLOOR SIDE BAR

A single longitudinal bar linking the bottom section of the Main hoop to the bottom section of the Front roll bar following the floor line.

5.2.5 DIAGONAL MEMBER

A transverse tube between a top corner of the main hoop and a lower mounting point on the other side of the roll bar.

5.2.6 A-PILLAR STRUT

A single bar joining the upper corner of the front roll bar to the lower point of the front roll bar, straight when viewed from the side of the vehicle.

5.2.7 FRAMEWORK REINFORCEMENT

A reinforcing member fixed to the roll cage to improve its structural efficiency.

5.2.8 REINFORCEMENT PLATE

A metal plate fixed to the body shell or chassis structure under a roll bar mounting foot to spread load into the structure.

5.2.9 MOUNTING FOOT

A plate welded to a roll bar tube to provide for bolting or welding to the body shell or chassis structure, usually onto a reinforcement plate.

5.2.10 REMOVABLE MEMBER

A structural member of a safety cage which is able to be removed.

5.3 SPECIFICATIONS

5.3.1 GENERAL COMMENTS

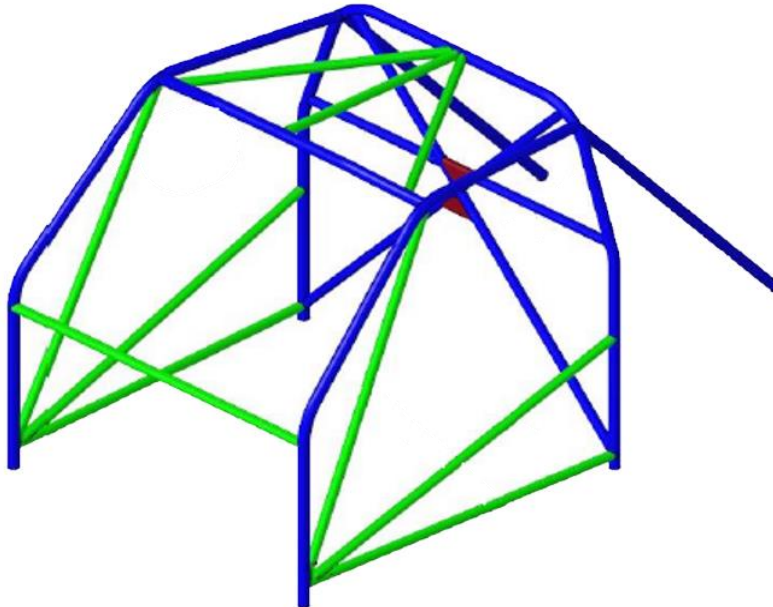
Safety cages shall be designed and made so that, when correctly installed, they substantially reduce body shell deformation and so reduce the risk of injury to occupants. The essential features of safety cages are sound construction designed to suit the particular vehicle, adequate fixings and a close fit to the body shell. Tubes shall not carry fluids or compressed air or be used as a pressure vessel. The safety cage shall be designed and fitted to minimise impact on ingress and egress of the crew.

5.3.2 TECHNICAL SPECIFICATIONS

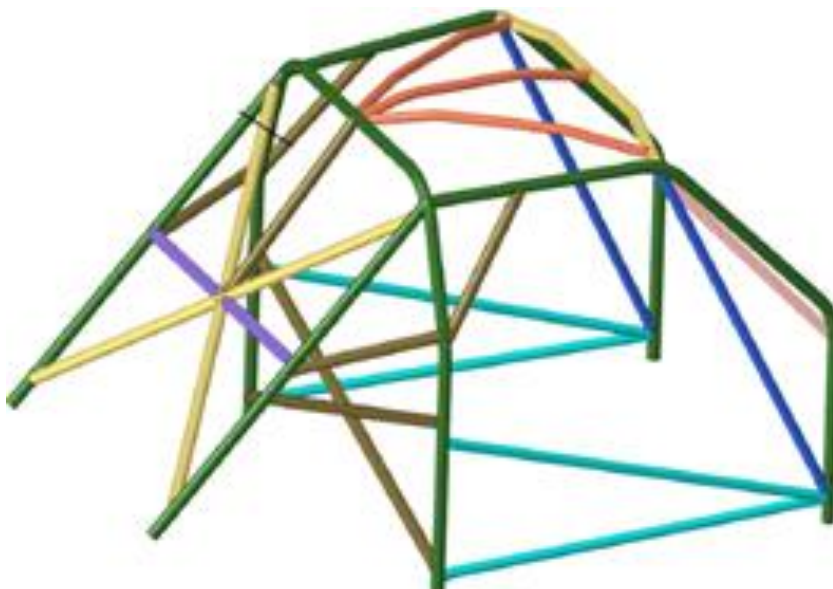
5.4 COMPLETE ROPS STRUCTURE OVERVIEW:

These rules allow for two options when building your ROPS cage. Cage specifications are predominantly the same, differences are noted below.

OPTION 1:



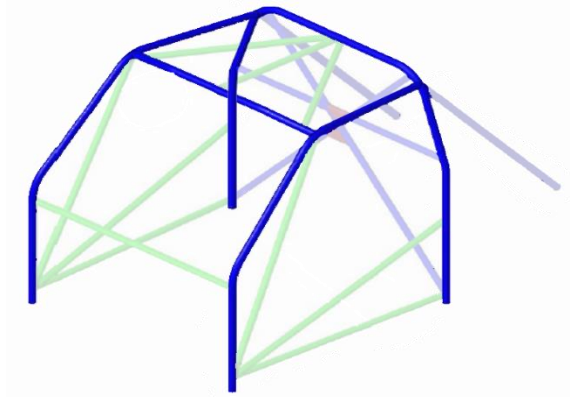
OPTION 2:



5.4.1.1 MAIN CAGE STRUCTURE:

Definition:

The basic cage structure, consisting of main hoop, front hoops and upper transverse (windscreen spreader) bar.



Minimum Material Specifications:

Diameter: 44.45mm

Wall thickness 2.5mm

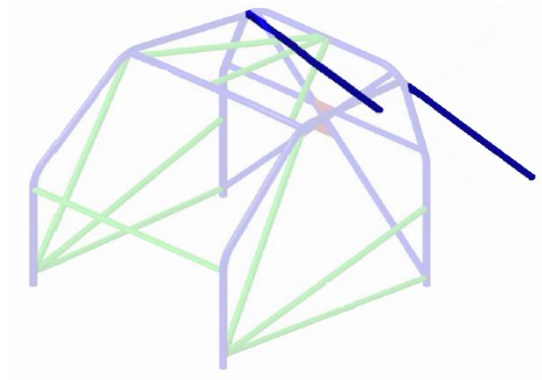
Notes:

- Main & front hoops shall be made in one piece without joints with smooth and even construction without ripples or cracks. The vertical part of the main hoop shall be parallel to the interior contour or the body shell where possible.
- The front leg of a front roll bar or of a lateral roll bar shall be straight, or shall follow the windscreen pillars and have only one bend with its lower vertical part.
- To achieve an efficient fixing to the body shell, the original interior trim may be modified around the safety cages and their fixings by cutting it away or by removing it.
- No part of the interior trim/ roof lining etc. should compromise the positioning of the ROPS.
- The roll bar shall be a minimum of 50 mm above any occupant's head in all vehicles. In conjunction with the vehicle's structure the roll cage should not leave unprotected any part of an occupant's shoulders when viewed from front or rear.
- The main hoop shall not overhang but shall be within 150mm of any occupant's head.
- An occupant's helmet shall be prevented from passing between the bars to the extent that the helmet is visible at the rear of the hoop when viewed from the side.
- No holes may be drilled in the main cage structure.

5.4.1.2 BACK STAYS:

Definition:

Straight reinforcing bars to support the main cage structure.

**Minimum Material Specifications:**

Diameter: 44.45mm

Wall thickness 2.5mm

Notes:

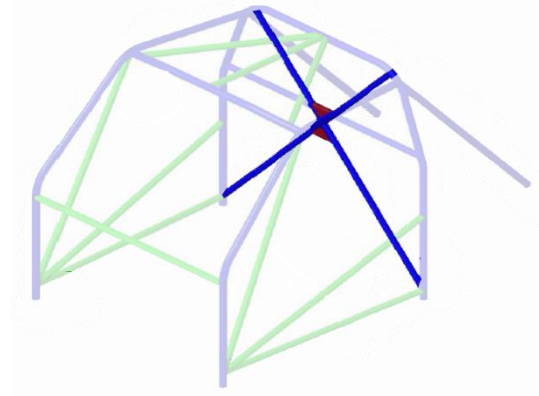
- Backstays shall be attached to the main hoop near the roofline and near the top outer bends of the main hoop on both sides of the vehicle. Backstays shall be attached to the main hoop within 100mm of the intersection of the diagonal cross brace. They shall be at angle of at least 30 degrees with the vertical, shall run rearwards and be straight and as close as possible to the interior side panels of the body shell.
- Their fixings shall be reinforced by plates. Each backstay should be secured by bolts having a cumulative section area at least two thirds of that recommended for each roll bar leg mounting, and with identical reinforcement plates of at least 6,000mm² area.
- A single bolt in double shear is permitted, provided it is of adequate section and strength and provided that a bush is welded into the backstay.
- No holes may be drilled in the backstays.
-

5.4.1.3 DIAGONAL CROSS BRACING:

OPTION 1

Definition:

Straight diagonal intersecting members reinforcing the main hoop.

**Minimum Material Specifications:**

Diameter: 44.45mm

Wall thickness 2.5mm

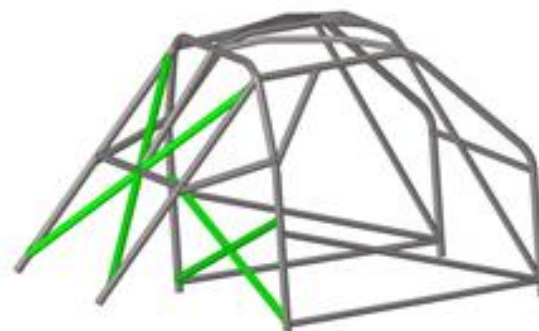
Notes:

- At least two diagonal members shall be fitted and shall be straight. The lower end of the diagonal shall join the main hoop or the backstay not further than 100mm from the fixing foot. The upper end must join the main hoop not further than 100mm from the junction of the backstay joint.
- Gussets shall be fitted on the upper and lower sections of the intersection from minimum 2mm Mild steel, roller along the horizontal profile and welded into the mid-point of the tube.

OPTION 2

Definition:

Straight diagonal intersecting members reinforcing the main hoop intersecting with the junction of the upper side bar on the main hoop. Plus a second cross brace in the backstays. The cross brace in the backstays also includes the straight bar from the top centre of the main hoop to the centre of the cross braces in the backstays.



Minimum Material Specifications:

Diameter: 44.45mm

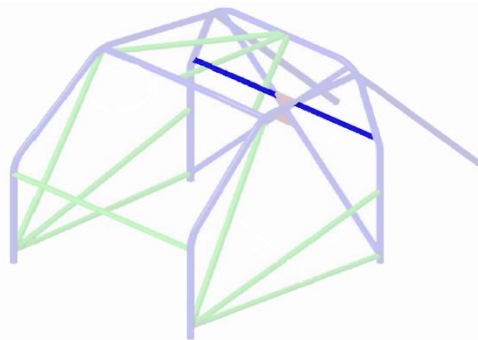
Wall thickness 2.5mm

Notes:

- At least two diagonal members shall be fitted in the lower section of the main hoop and shall be straight. The lower end of the diagonals shall join the main hoop not further than 100mm from the fixing foot. The upper end must join the main hoop not further than 100mm from the junction of the side intrusion bar.
- At least two diagonal members shall be fitted in the backstays and shall be straight. The lower end of the diagonals shall join the backstay not further than 100mm from the fixing foot. The upper end of the diagonals must join the backstay bar not further than 100mm from the junction of the main hoop.
- Gussets shall be fitted on the upper and lower sections of the intersection from minimum 2mm Mild steel, rolled along the horizontal profile and welded into the mid-point of the tube.

5.4.1.4 HARNESS BARS:**OPTION 1****Definition:**

Horizontal bar joining the main hoop to the diagonal cross braces to support the main hoop and retain the harness vertically.

**Minimum Material Specifications:**

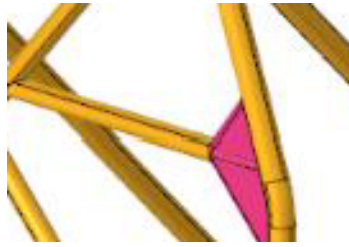
Diameter: 44.45mm

Wall thickness 2.5mm

Notes:

- The harness bar must be placed level with or within 50mm above the shoulder height of the occupant allowing the harness to pass over the bar and forward in a near horizontal plane before contacting the shoulders.

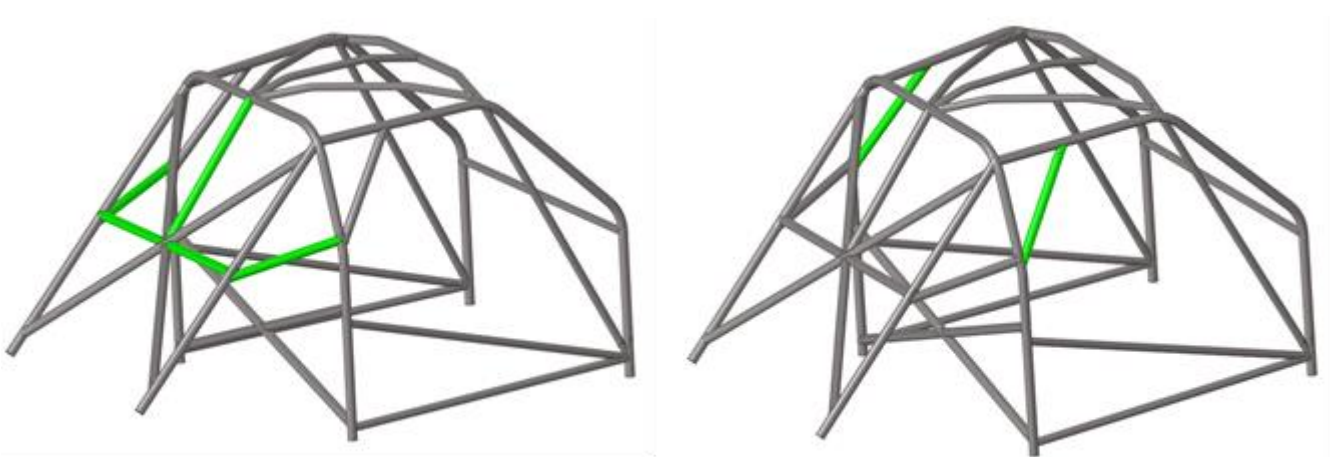
- Optional gusseting of the harness bar to the main hoop (as shown below) is recommended to further improve side impact load strength.



OPTION 2

Definition:

Horizontal bar joining the main hoop to the diagonal cross braces to support the main hoop and retain the harness vertically.



Minimum Material Specifications:

Diameter: 44.45mm

Wall thickness 2.5mm

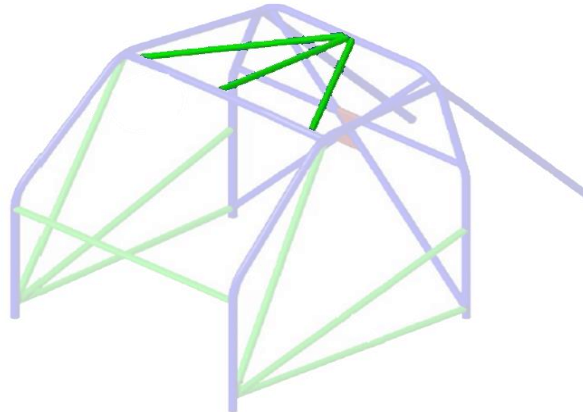
Notes:

- The harness bar must be placed level with or within 50mm above the shoulder height of the occupant allowing the harness to pass over the bar and forward in a near horizontal plane before contacting the shoulders. The harness bar is mounted to the backstays of the main cage structure to allow clearance inside the cabin.
- Optional gusseting of the harness bar to the main hoop is recommended to further improve side impact load strength.

5.4.1.5 ROOF REINFORCEMENT:

Definition:

Reinforcing bars to support the roof section and upper front hoop corners.

**Minimum Material Specifications:**

Diameter: 38mm

Wall thickness 2.5mm

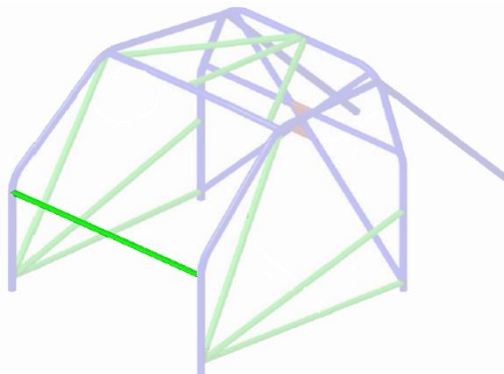
Notes:

- Roof reinforcement shall consist of:
 - At least one longitudinal bar down the centre joining the upper windscreen spreader bar to the main hoop
 - Two diagonal members joining the outer front corners of the upper windscreen spreader bar to the centre of the main hoop roof bar.
- These members may have bends up to 20 degrees to allow bars to fit closer to the roof line maximising occupant headroom. Each member must be fitted so they appear straight when viewed from above.

5.4.1.6 LOWER WINDSCREEN SPREADER BAR:

Definition:

Straight horizontal member to support front hoops.



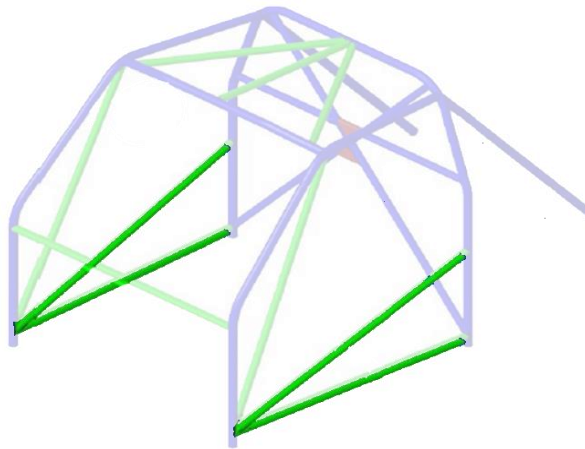
Minimum Material Specifications:

Diameter: 38mm

Wall thickness 2.5mm

Notes:

- The lower windscreen spreader bar shall join the two front hoops. The member shall be straight and attached to the front hoops within 150mm above or below the lower most bend.

5.4.1.7 SIDE INTRUSION AND LOWER FLOOR SIDE BAR:**Definition:**

Straight reinforcing bars to support the main hoop and provide side intrusion protection. These members also provide significant “Punch Through” strength adding to the main and front hoop foot plates. The lower floor side bar also provides strength into the lower front hoop in the event of contact with a tree or other immovable object in the lower firewall section of the vehicle.

Minimum Material Specifications:

Diameter: 38mm

Wall thickness 2.5mm

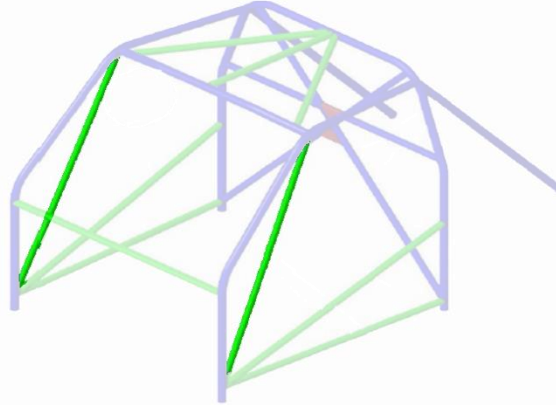
Notes:

- Lower Floor Side Bar – the lower floor side bar shall be fundamentally straight, attached to the main hoop and front hoop within 150mm of the foot plate. This bar may be bent up to 20 degrees at one or both ends to allow fitment however the majority of the member shall follow the floor.
- Side intrusion bar – the side intrusion bar shall be joined to the main hoop and front hoop above the lower floor bar. It may be either straight or bent to follow the seat line. If bent the side intrusion bar must have a reinforcing member to the lower floor bar in the centre third of the bar. The rear attachment must be at the centre or above of the lower vertical section of the main hoop. The side intrusion bar shall be mounted as low as possible to the front hoop to minimise impact on occupant access.

5.4.1.8 FRONT HOOP SUPPORT:

Definition:

Primary reinforcement to add rollover capacity to the upper corners of the A pillar and front hoop.

**Minimum Material Specifications:**

Diameter: 38mm

Wall thickness 2.5mm

Notes:

- The front hoop reinforcement member shall be fitted to both sides, attached to the horizontal section of the front hoop within 100mm of the upper windscreen spreader bar. The lower section attached to the lower most section of the front hoop generally at the junction of the lower floor bar or foot plate or both. If the front hoop support intersects the side intrusion bar it must be sectioned and straight when viewed from the side. The member may be bent by up to 20 degrees however must be fitted so the bar appears straight when viewed from the side.

5.4.2 OPTIONAL REINFORCEMENT OF THE ROLL CAGE

Recommended optional members consist of reinforcing gussets on the harness bar to main hoop junctions and a single vertical strut from the centre of the diagonal cross brace to the centre of the main hoop horizontal section. These members/gussets provide additional support and are especially recommended in vehicles over 3200kg

Other optional reinforcing members are allowed however must be manufactured out of 44.45 or 38mm tube as per the specifications contained in this schedule and shall be fitted in a way to minimise impact on occupant access and/or injury risk.

5.4.3 REINFORCEMENT OF BENDS AND JUNCTIONS

The junction of the main hoop or the front roll bar, the top rear bends of the lateral roll bars and the junction between the main hoop and the backstays may be reinforced with longitudinal struts.

The ends of these reinforcing struts shall not be more than half way down or along the members to which they are attached, except for those at the junction of the front roll bar, which may join the junction of the door strut/front roll bar.

5.5 FIXING OF ROLL CAGES TO THE BODY SHELL

Roll cages shall be fixed to the vehicle by one of the following three options:

5.5.1 OPTION 1

As a minimum, fixings shall comprise:

- One for each leg of the main or lateral roll bar;
- One for each of the front roll bar;
- One for each backstay.

Fixing may be by bolting or welding to the body shell.

Each fixing plate of the front, main and lateral roll bars shall include a reinforcement plate 3mm thick, or the gauge of the tube onto which it is welded, whichever is the greatest.

If bolted to the body shell each fixing plate shall be attached by a minimum of three bolts on a steel reinforcement plate at least 3mm thick and of at least 15,000mm² area which is welded to the body shell. This area can be made up of a single plate or by adding a third dimension (i.e. angle) to its profile. Minimum Base Plate to be at least 10,000mm² with a 100mm single dimension.

Bolts shall be either M8 size to ISO standard 8.8, hexagon head high tensile fasteners to AS2465 or capscrews to AS14201 with nuts to AS1112 or better. Pins for removable connections shall be the same strength specifications as the bolts.

Fasteners shall be self-locking or fitted with lock washers. Additional fasteners may be used however all fasteners shall meet these requirements.

If welded to the body shell roll bar legs shall be welded to reinforcement plates. Roll bar feet shall not be welded directly to the body shell without a reinforcement plate.

5.5.2 OPTION 2

Alternatively all the attachment points of the roll cage may be fitted with a base plate and lower plate complying with the table below. The base plate, complying with the area requirements shown in the table below, may be welded to the body shell, in which case the use of bolts and the lower plate is not required.

Application	Minimum Area	Minimum single dimension
Upper (base) plate Over 1151kg	15,000mm ² Min Baseplate of 10,000mm ²	100mm
Lower plate	4,500mm ²	Proportional to upper plate

5.5.3 OPTION 3

Where Event Group regulations permit, the roll bar protection may be an integral part of a space frame tubular chassis. The roll cage shall comply with these regulations from a point above where the predominately vertical portion of the roll cage meets a predominately horizontal portion of the chassis. Parts of the roll cage may extend below this horizontal plane and become integral with the chassis. Vertical components shall be braced from the chassis.

5.6 PROTECTIVE PADDING

All sections of the roll cage that could come into contact with occupant's bodies or helmets shall be provide with non-flammable padding for protection.

5.7 REMOVABLE MEMBERS

Demountable joints are not recommended in any members, any demountable joints must carry certification of a level higher than this schedule and we recommend you discuss your plans with the chief scrutineer prior to purchasing/fitting any demountable joints.

Demountable joints shall not be used as part of the main, front or lateral roll bar, or the junction of the backstays to the main hoop.

5.8 GUIDANCE ON WELDING

All welding shall be of the highest possible quality with full penetration and preferably using a gas shielded arc, and shall demonstrate good workmanship.

When using heat-treated steel the special instructions of the manufacturers shall be followed

5.9 MATERIAL SPECIFICATIONS

All tubing used in roll bar protection structures shall be circular section cold drawn steel tube (CDS) with properties as shown in the following table:

Composition	Property Specification
Carbon content	0.3% max.
Manganese content	1.0% max.
Other alloy content	0.5% max.
Tensile strength	350 MPa min.

These figures represent the minimum permitted specification. In selecting the steel, attention shall be paid to obtaining good elongation properties and adequate weldability. The tubing shall be bent by a cold working process and the centreline bend radius shall be at least three times the tube diameter. Where the tubing is distorted during bending, the ratio of major to minor diameter shall be 0.9 or greater.

5.10 FLEXIBLE JOINTS

The use of flexible mounts shall be only permitted on the backstay lower mounting point provided the following minimum specifications are met:

5.10.1 OUTER SLEEVE

The material shall be that used for the rear brace or of a higher specification and shall be welded to end of rear brace.

The wall thickness shall be 2.5mm minimum

The minimum outside diameter shall be that used for the rear brace and the maximum outside diameter shall be 25% larger than that used for the rear brace.

The minimum length shall be equal to the outside diameter of the rear brace.

5.10.2 BUSHING MATERIAL

Bushings may be rubber, urethane, nylon or similar flexible material

Bushing outside diameter shall be no greater than three and a half times the outside diameter of the inner sleeve.

The minimum length of bushings shall be equal to the length of the outer sleeve.

5.10.3 INNER SLEEVE/SPACER TUBE

Inner sleeves shall be steel of minimum thickness of 1.0mm.

The inside diameter of inner sleeves shall equal the outside diameter of the cross bolt.

Inner sleeve length shall be no greater than the length of the bushing material, and at least 200mm longer than outer sleeve.

5.10.4 CROSS BOLT

Cross bolts shall be 11mm or 7/16 inch, minimum diameter and at least 8.0 Grade steel.

5.10.5 FIXING LUGS

Fixing lugs shall be steel with a minimum thickness of 5mm and a minimum length no less than the outside diameter of the bushing material.

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SECTION 6 – SCRUTINEERING



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6 SCRUTINEERING

6.1 GENERAL COMMENTS

6.1.1 PURPOSE

The scrutineering process shall ensure that vehicles comply with class rules, and that each vehicle and its equipment is in a safe condition, and that all vehicles commence competition in an equal state of preparedness.

It is the intention of the event scrutineering team to check log books and have each Competitor's vehicle complete the scrutineering process successfully.

Competitors can expedite the event scrutineering process by attending at the location and time advised in the Event Supplementary Regulations.

Log books are to be kept up to date and any incident that affects the safety of the vehicle will require the driver to have the vehicle re-scrutineered.

Any modifications to the vehicle will require the vehicle to be scrutineered prior to the event and have the log book updated.

The CCDA can, at any time, call for a car to be re-scrutineered. The car shall not be raced until such time as it passes the re-scrutineering.

6.1.2 CLASSES

The vehicle classifications shall be read in conjunction with Event Supplementary Regulations.

Vehicles displaying a current registration certificate issued by the relevant authority in their State, Territory or Country of origin shall be deemed to meet any requirements of these rules to be road registered.

6.1.3 SAFETY

A general safety check shall be performed on all competing vehicles and shall include, but not be limited to the following specific items.

6.1.3.1 DRIVING LIGHTS:

Lighting shall be in line with class regulations.

6.1.3.2 WHEEL SPACERS:

In Standard, Touring, Production and Challenge Class vehicle specifications, wheel spacers are not permitted. A wheel spacer, or hub extension is any device that is fitted between the original hub and the wheel centre to change wheel offset distance.

6.1.3.3 RECOVERY KIT:

The rules call for the rating of all mounted and portable recovery equipment to be equal or greater than the capacity of the winch and/or the weight of the vehicle, up to twice the vehicle weight.

6.1.3.4 RECOVERY POINTS:

Recovery points constructed as a plate and a loop of steel welded to it, as fitted to Nissan and Mitsubishi vehicles, and the eyes on commercial bullbars, unless they are triple laminated and fully welded, are not permitted. Tie down points including those on monocoque body vehicles will not be accepted.

Rated towbars will be accepted as recovery points, however tow balls shall not be used for connecting recovery equipment.

Recovery points should be suitably over engineered. The rules call for recovery points to be rated at twice the vehicle mass. While recovery points may not be test loaded scrutineers will be applying the “over engineered” approach when inspecting them at scrutineering and the onus to supply proof of strength will be with the Competitor.

6.1.3.5 CABLE STORAGE HORNS/PEGS:

Cable horns or pegs that protrude in a manner that might injure or impale the winch operator should he fall against them will not be acceptable. The minimum requirements are 12mm diameter round rod/pipe with a minimum outside bend radius being 50mm.

6.1.4 ROLL BARS

Refer to the section on Rollover Protection in Section 5 of this Manual for full details. There may be a moratorium in place with new regulations coming into force so check with the Supplementary Regulations for any variations. The scrutineering team will inspect Rollover Protection thoroughly, and will evaluate compliance on an individual basis.

6.1.5 FIRST AID KITS

Competing vehicles shall carry an adequate outdoors first aid kit in or on the car at all times and it must be readily accessible.

6.1.6 CRASH HELMETS

Crash Helmets shall be in acceptable condition without deep scratches or abrasions and shall carry AS 1698 rating sticker. Helmets which have been painted, or which carry large stickers, will not be acceptable.

6.1.7 TYRE SIZE

A tyre gauge will be used to measure tyre height and width. Please make sure that your tyres fall within the specified range. In touring class, a 12.5-inch section tyre mounted on a 10-inch rim may not comply. The gauge must be able to move freely over the diameter and width of the tyre.

In events that are not on closed property wheel arch flares will be expected to cover entire tread of tyre, and it is strongly recommended that rear mudflaps also cover the entire tread width to protect following competitors from track debris. On vehicles with short rear overhang, rear mudflaps should be positioned so that their bottom edge is at or below a line drawn at 45 degrees from the point of tyre contact with road.

6.1.8 SUPPLEMENTARY FUEL STORAGE

Fuel containers (other than tanks) shall be of an approved type.

Auxiliary tanks will be inspected for safety. Custom built tanks shall be manufactured and supported to similar standards as those of the recognised after-market tank manufacturers.

6.1.9 INTERIOR ACCESSORIES

Any vehicle equipped with SRS airbags shall not have any accessories (GPS, map lights, etc.) located in such a position that it may cause the accessory to be caught between the SRS airbag and the driver or passenger.

The rule pertaining to Video camera mounting will not apply to cab chassis type vehicles; however, any camera shall be mounted so as to minimise the possibility of bodily contact in the event of a rollover or collision.

6.1.10 ROOF RACKS

A roof rack could constitute a danger in the event of a rollover. They also add considerably to the height of the centre of gravity. Please be aware of these dangers if considering the use of a rack.

6.1.11 LOOSE ITEMS IN VEHICLE CABINS

Any loose items other than soft recovery straps will not be acceptable in the driver/co-driver compartment.

6.1.12 READY TO START CONDITION

The scrutineering team shall check ALL items. It is the CREWS RESPONSIBILITY to rectify any transgressions and seek final scrutineering approval prior to commencement of the event.

Vehicle, all relevant documentation, and all compulsory equipment shall be present at event scrutineering. Recovery equipment, spares, tools, personal gear, garbage bags, water and other nominated items in the supplementary regulations shall all be securely stored but available for inspection.

6.1.13 UNACCEPTABLE EQUIPMENT

Equipment may be rejected because of its particular construction and its potential to fail.

For example a snatch block that has a relatively shallow machined groove for the cable, a flat area each side of the groove, and excessive clearance between the sheave and cheek plates could cause the cable to ride out of the groove (via the flat area), jam between the sheave and one cheek plate, and force the retaining circlip to shear. This has been observed to cause the cheek plate to separate from the axle, risking pieces of the mechanism to be ejected with considerable force.

Recovery equipment must carry the appropriate rating

6.1.14 CLASS COMPLIANCE COMMENTS

Please read the class rules very carefully. If you have any doubt as to the compliance of your vehicle, contact the Chief Scrutineer or the Event Organiser.

6.1.15 SCRUTINEERING PENALTIES

Any vehicle that fails the event scrutineering shall not compete until all transgressions are rectified and final scrutineering approval has been given.

Any team which fails to turn up to scrutineering prior to the cut off of the allocated time period shall not take part in the event at the discretion of the Event Organiser

MANUAL OF 4WD MOTOR SPORT
SECTION 7 – MARSHAL ROLES AND RESPONSIBILITIES



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7 MARSHAL ROLES AND RESPONSIBILITIES

Officials and Marshals make a valuable contribution to Four Wheel Drive Motor Sport and the degree to which they are trained and briefed before an Event can have a direct bearing on the success of the Event and the enjoyment of the sport by all participants.

Officials and Marshals need to make mature, considered and informed judgements within their range of responsibility.

The provisions of the Standing Regulations with respect to abuse of alcohol and drugs applies to Officials and Marshals as equally as it does to Competitors.

Officials and Marshals shall be familiar with the Standing Regulations, Sporting Regulations and the Supplementary Regulations for the Event that they are adjudicating.

It is the Event Organisers Directors responsibility to ensure that all Marshals used on the event are appropriately trained and are CCDA members.

7.1 MARSHAL ROLES

Note that one or marshal roles may be undertaken by a single Marshal. The Stage Manager shall ensure that Marshals are able to adequately perform their allocated role/s.

7.1.1 STAGE MANAGER

The Stage Manager shall supervise and brief all Marshals on the Stage and ensure that:

- All Stage Marshal positions are filled with appropriately trained personnel.
- A copy of the current CCDA Standing and Sporting Regulations are on site.
- The course (Stage) is previewed and has been “driven” prior to first competing vehicle.
- Bunting and pegs are in the correct position.
- The course (Stage) layout is defined with start and finishing boxes or markers (for flying finish).
- The first marker for DNF points is in place past the first obstacle, or if no marker is to be used that the DNF point has been determined prior to start of the Stage.
- All required equipment is with each Marshal.
- Radio communications work throughout the course.
- Start, Finish and Track Marshals know and follow the correct procedures.
- The information on each score sheet is complete and correct once entered;
- The completed score sheets are taken to the official scorer immediately after closing of the Stage or as early as is practical.
- All pegs are to be collected and returned to the property officer or other person/place as required.
- All rubbish is removed from the course and all bunting is removed and disposed of appropriately after the Stage is completed.
- The Stage Manager must have a firm understanding of any conditions specific to each event including but not limited to conditions such as:
 - Flying finish/Finish Garage - as per Timing Marshal
 - Parc ferme

7.1.2 TRACK MARSHAL

The Track Marshal shall:

- Observe the progress of the competing vehicle to verify that the Crew is driving within the rules.
- Constantly assess the Stage to ensure clear passage and immediately alert the Stage Manager should a problem arise.
- Observe correct self-recovery procedure is being followed and apply appropriate penalties when warranted
- Ensure the use of the Penalty Warning System is employed where required by giving one long whistle (or air horn) and an explanation that a warning has been given. An example would be the whistle then yelling "Cable Dampener" to alert the competing team to the nature of penalty being warned of. Penalties will then apply if an infringement for which a warning has been given is continued or repeated.
- If necessary, stop all competition activities on the Stage in the event of immediate danger and alert the Stage Manager of the circumstances only allowing the Crew/s to continue or restart if the danger has been eliminated. For example, if a tree is in danger of falling or there has been a collapse in the track.
- Ensure that once the Stage is started, it is not materially altered. However if track altering takes place by a Crew during competition, such as removing or placing rocks, it is not the responsibility of the Track Marshal to reinstate the track as it was before it was altered.
- Report any track markers that are dislodged to the Recorder. Replace or repair any track markers that are dislodged or broken before the next Crew starts the Stage.

7.1.3 TIMING MARSHAL

The Timing Marshal shall time each vehicle as it travels through the Stage including;

- Starting countdown and stopwatch activation when a vehicle is started.
- Stopping the stopwatch when a vehicle finishes the course.
- Be aware that there could be a "flying" or "finish garage" finish;

The Timing Marshal shall:

- Know the DNF time but not stop the watch in the case where a Competitor exceeds the DNF time until advised by the Stage Manager.
- Be ready for the Starter's advice that the Crew is ready.
- Be located at the finish point of the Stage.
- Ensure the competing vehicle clears the Finish Garage area promptly.

The Stage Manager shall have two people with a stopwatch each to time the Stage. The time recorded on the primary stopwatch is to be used in the first instance with the time recorded on the second stopwatch to be used only in the event of a failure of the first stopwatch.

In the Event that both stop watches fail the Crew may be restarted or may take another run on the Stage.

7.1.4 RECORDS MARSHAL

The Records Marshal is in charge of all paperwork. The Records Marshal Shall:

- Check for the presence of all Competitors from that day's Group or Event Competitor List.

- Ensure that they have completed a Performance Record sheet and other Event specific record documents for all Competitors listed to run the Stage. This will include Competitors who:
 - Successfully finish the Stage, who would record a time and/or distance and may earn points and/or incur penalties;
 - Start but fail to finish the Stage within the DNF time, who would record the applicable DNF. It is important to record both the time taken AND the DNF to avoid subsequent dispute.
 - Start but fail to finish the Stage, who would record the applicable DNF;
 - Start but fail to pass the qualifying marker, who would record the applicable DNS;
 - Jump the start who would record a 10 second penalty as well as any further points, distance or penalty records. If the jump start is deemed deliberate a DNF would be recorded.
 - Are present but fail to start the Stage, who would record the applicable DNS;
 - Have withdrawn from the Stage, who would record a DNS; (Not ready to start in allocated order)
 - Have withdrawn from the Event, who would record a DNS; (Not ready to start in allocated order)
 - Have been disqualified from the Stage or the Event, who would record a DNS; (Not ready to start in allocated order)
 - Simply don't appear and whose Event status is unknown, who would record a DNS. (Not ready to start in allocated order)
- Record all relevant information as required and including:
 - Vehicle ID;
 - Time/Timing;
 - Points earned;
 - Penalties recorded;
 - DNF or DNS;
 - Disqualification
- Ensure a Crewmember signs the Stage Performance Record
- Ensure that Stage Performance Records are distributed as required i.e. the top original to the Crew, the second sheet to the Stage Manager for verification and passing on to headquarters for recording of points and the third sheet to remain in the book with the Records Marshal.

Once the Stage Performance Record is signed, no alterations are to be made without the Crew's knowledge. Any agreed changes are to be initialed by the Records Marshal and the Crew and notified to the Stage Manager.

7.1.5 START MARSHAL

It is the responsibility of the Start Marshal to:

- Co-ordinate the Start of the Stage.
- If there is a Timing problem to take all reasonable steps to restart the competition vehicle.

- Call up teams in the correct order.
- Always have at least two teams ready to commence in correct order and one team in the garage ready to start.
- Check harness/seatbelt for correct fitment by the Crewmembers and have situation rectified if incorrect.
- Check helmet & chinstrap for correct fitment and have situation rectified if wrong
- Clear cars at start line to give recovery Crew access in event of incident.
- Check with the Crew that all gear is stowed as per rules i.e.; shackles, straps etc. and have situation rectified if incorrect.
- Check track is visually clear and seek a radio check for “all clear” by Track Marshal/s.
- In the event the driver moves the vehicle forward or backwards prior to the start of the timing advise the Recorder of this as penalties will apply for jumping the start.

7.1.6 FIRST AID OFFICER

The First Aid Officer is under control of the Competition Manager and shall:

- Possess a current Level 3 First Aid Certificate.
- Have all necessary First Aid Equipment.

MANUAL OF 4WD MOTOR SPORT
SECTION 8 – LOGBOOK PROCEDURE



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8 LOG BOOK PROCEDURE

Information sheets available from CCDA or their web site

- Log Book Application Form
- Roll bar certification
- Scrutineers report

Owner fills in details on application forms for vehicle

- Personal details
- Vehicle details
- 2 photo's 1 front, 1 side view of vehicle
- Has the roll bar manufacturer fill in ROPS compliance sheets?
- Has a CCDA scrutineer inspected the vehicle & submitted the signed report when the vehicle passes the inspection for the specific CCDA Vehicle Class?

Scrutineering report, ROPS Certification, log book application and photo's to be sent to CCDA at:

Email: info@ccda4wd.com.au

or **(by prior arrangement)** bring them to the next event for log book and CCDA ROPS Label issue. The label shall be affixed to the roll cage above the drivers (i.e. right hand) door, in clear view for scrutineering inspection.

The CCDA enters details into log book, checks photos and ROPS information and establishes a vehicle file. The log book is then returned to the driver prior to the event or in the case of issuing at the event the log book is returned to the driver at the event.

NOTE: You cannot scrutineer your own vehicle.